

**Falmouth Planning Board**  
**Select Board Meeting Room, Town Hall – January 13, 2026**  
**Meeting - 6:30 pm**  
**MINUTES**

*Present:* John Druley, *Chair*, Charlotte Harris, Robert Leary, Paul Dreyer, Jim Fox, and Thomas Zine  
*Also Present:* Jed Cornock, *Town Planner*; Melinda Tondera, *Assistant Town Planner*

Chair Druley opened the meeting at 6:30pm.

**PUBLIC COMMENT**

None at this time.

**MINUTES: December 30, 2025**

**MOTION** by T. Zine/R. Leary to approve the December 30, 2025, meeting minutes, as presented.  
**Voted 6-0-0.**

**PUBLIC MEETING:**

*Applicant:* ASD Cape Cod Holdings, LLC - Site Plan Review application to install large-scale ground-mounted solar arrays at 41 , 42, 48 and 59 Theatre Dr & 0 Boxberry Hill Rd. Maps 1003001 163,1701 028B014,1701 028039, 1701 028A015, 1004007035.

Matthew Terry, Ament Klauer LLP – the applicant met with the Fire Chief and Town Staff to discuss the project. The Fire Department issued a referral since that time. A site visit was conducted yesterday for the benefit of the Board.

M. Terry – a memo was filed on December 23<sup>rd</sup>. The letter went through some of the questions brought up at the December 9<sup>th</sup> hearing on a point-by-point basis. The first question was with respect to emergency response plans and if a fire retardant will be used on the site. The response is that the emergency response plan will be worked on with the Fire Department. General industry consensus on dealing with battery fires is that the enclosures are designed to burn in place without propagation. Thus, fire departments should engage defensively rather than offensively through the use of water or foams. Using water as a cooling agent on the enclosure would be ineffective and should be avoided.

The second question was if the access roads be wide enough for emergency response staging. The response was that the access roads will be designed and installed to meet emergency access requirements and will be designed in consultation with the Fire Department.

The third question is if the surrounding residences will need to be put under an evacuation order due to a gassing or venting event and the response is no. The emergency exclusion zone perimeter that would be set up in the event of any kind of gassing or venting event would be approximately 100'-150'. During an emergency, there will be air quality monitoring. Depending on certain conditions, the perimeter may need to be extended to 250'-300'. This would not place the exclusion zone overlapping any residence that would require evacuation. In the event of an extension, property owners might receive advice to close windows and doors. The closest residence to any of the three equipment pads is approximately 500'.

J. Druley – the fourth question can be skipped for now.

M. Terry – in terms of the fifth question, with respect to the White River Project, which is in New York, is it accurate to state that the environmental impact remains under investigation and that the extent of the impact is unknown? The answer is that the question was misleading and not accurate. The White River fire did not involve the combustion of any solar materials or damage to the project equipment. It was a brush fire that burned grass and wood. The minimal damage that did occur to the appliance equipment was heat damage to the cable coverings of a few modules that were temporarily stored on the ground, but the modules themselves did not burn.

For the sixth question, has the applicant considered whether they would be willing to install one or more Town wells as part of the project. The response is that, as the recipient of a donation of property from the applicant, the Town is the appropriate party to contact concerning the installation of wells. The Town may be willing to consider installations and locations on the property. These would not be subject to the solar array ground leases entered into with entities. Discussions with the Town would be outside of the scope of the site plan review. The three ground leases for the three projects are expected to cover approximately 60 acres of 137 acres that comprises the property, subject to this application.

J. Druley – I would like to address the seventh question later.

M. Terry – for the eighth question, does the applicant have power agreements with Eversource? The applicant does not have power agreements but does have interconnection service agreements with Eversource which govern the interconnection of each of the projects.

The ninth question is if the Wildlife Corridor Overlay District portion of the Zoning Bylaw restricts having fencing on site? The answer is that the provision of the Bylaw does not prohibit all fencing within the Overlay District. Corridors cannot be restricted by fencing. On consultation with Town staff, the applicant has proposed several corridors throughout the entire property and designated open space, including approximately 42 acres of upland and a gap in the fencing of the west array project. None of these corridors will have fencing across them that would pose a barrier to wildlife movement which complies with the Wildlife Overlay portions of the Bylaw.

The tenth question is, with respect to construction access, would the applicant commit to not using Boxberry Hill Road to access the site and instead use Route 151. The applicant would be willing to use Route 151 to Ranch Road as the primary access to the site for construction purposes. We would need to use a portion of Boxberry Hill Road as it provides the only roadway access to the project along that stretch. I

For the eleventh question, has the applicant considered what effect the project will have on bird life? These projects will provide significantly increased and diverse avian habitat and nesting areas, both manmade and natural, throughout the project sites. In addition, the applicant has voluntarily committed to no tree cutting between April and August of any year to avoid potential impacts during the bird nesting season.

In terms of question twelve, where will parking for the sledding hill be located? The response is that the land comprising the sledding hill is being donated to the Town. This area will be within the Town's

custody and control. The Town may provide parking at its discretion. It is the applicant's understanding that public parking currently exists to the west of the sledding hill.

For the thirteenth question, at the applicant's first hearing, the extent of savings for local residential subscribers was discussed and the applicant responded that it would provide concrete figures based on average usage. There will be two programs, including a 20% discount, which is not an income-qualifying discount, and will be available to any consumer of electricity in Falmouth. If you assume an average annual electricity bill of \$3,450 this would provide an annual savings to that consumer of \$621. Over the 20-year initial term of the array, which could be extended, this would be a savings of \$12,420. The second program is a 100% discount on the supply portion for qualifying low-to-moderate income customers. For the same annual electricity bill of \$3,450 this would be a 100% savings on the bill or \$62,100 over the 20 year term. Each individual consumer's bill will differ.

Henry Dick, 392 Boxberry Hill Road and representing the Pond Association – we discussed many points with the applicant, including a walking trail along the east side of the lake. The applicant has proposed utilizing an existing golf cart track, though the Association would prefer to use an existing trail area and to expand upon it. There is an existing parking lot on Town land which could be used for this trail. The applicant should work with the ConsCom and the Pond Association regarding the location of this trail.

C. Harris – who will be responsible for maintenance of the trail?

H. Dick – there should not need to be much maintenance.

Ken Jones, Pond Association – a sum of money was agreed to with the applicant which will be used for testing nutrient levels in the lake on a quarterly basis.

David Leslie, Precinct 1 – I convey my support for the project. I would like to be reassured that the issue of noise pollution has been adequately addressed. Will noise mitigating panels be installed?

Glenn Kelly, Precinct 2 – will the walking trails meet ADA compliance?

Dave Moriarty – could there be a wooden fence instead of a metal fence to hide the panels from being visible? It will be important for a noise study to be completed.

Carey Murphy, Precinct 7 – what is the liability for the Town from this project if there is an emergency?

H. Dick – elimination of one array would greatly improve the sight lines for the project.

D. Moriarty – the other solar arrays do not have battery backup. I believe these will be noisy for the site. I also saw a recommendation that there be a water source on site.

J. Druley – I researched and read a pamphlet from the MA Department of Energy Resources, MA DEP, and Clean Energy Center. This may answer a lot of people's answers. Much of the information included was very positive.

C. Harris - the Board is seeking some flexibility as to the finished project. A condition to retain the trees along the road would be of interest. This would lead to the proposed berm being located further back. This would improve the quality of the project for residents.

The applicant team explained that the tree line on the perimeter is proposed to be preserved as much as possible. Some trees around the proposed driveways will need to be removed. Additional plantings will also be included.

Raul Lizardi, Cape & Islands Engineering – starting from the existing ground level, the applicant is targeting a 10' height. The upslope will cover 30', then there will be a plateau of 4', and a downhill to meet the existing grade which could be another 30'. This will be a width of 60'-65' in some areas. The widths will vary depending on the grades. The existing vegetation will be protected as much as possible. The earth berm will create a visual barrier and then the vegetation will add an additional visual barrier.

C. Harris – could this be a deeper setback than 100'?

R. Lizardi – some of the fences will have a much deeper setback. The east and west arrays will have a selection of trees that need to be removed. The existing trees can be graded outside of, in order to preserve them.

C. Harris – we would like to discuss removing the poles along Boxberry Hill Road and burying the wires.

J. Fox – I think the black chain-link fence will disappear more so than a wooden fence.

T. Zine – I would like to know what will happen with the benches that have been dedicated throughout the site.

M. Terry – conversations can be had with the current property owner regarding the benches.

T. Zine – will the existing irrigation system be removed? Will that require an environmental review if the pipes are lead or asbestos lined?

The application team stated that the plan is to remove the existing irrigation system. It is unclear what this is made of, and this may determine if additional testing is needed.

J. Druley – in the Site Plan Review regulations, it states that all utility connections from the installation should be underground, if reasonably possible.

The applicant team explained that there will be eight new poles and pad-mounted equipment.

J. Druley – why can't the connections be run underground to Sandwich Road?

The applicant team explained that Sandwich Road is quite a distance away and there are existing poles on the street. It is not a reasonable effort to ask that the existing poles be placed underground by the applicant. This costs \$3M per mile to complete. Burying existing poles is not in the spirit of the regulations.

J. Druley – this would be a safety effort for the community. It is only a 0.5 mile area to complete. I care more about the wires than the poles being underground. This may be made a condition of approval. Eversource wants the items underground.

The applicant team explained that this is not a safety item, as the poles are already existing. The Board would be requiring the applicant to replace and improve existing poles. This is not in the spirit of the bylaw.

J. Druley – won't the project send more voltage to the poles?

The applicant team stated that this would be the same voltage to the poles. This is a misunderstanding of the project.

M. Tondera – the intent of the bylaw is to keep anything on site underground, not existing items offsite. This could be a due process violation due to lack of substantial evidence, and the Board should move on from this discussion.

P. Dreyer – the Board received a letter from a local toxicologist regarding the proximity to golf courses and a risk of Parkinsons. It encourages this project as a net environmental benefit based on this study.

T. Zine – we received a Fire Department referral. The applicant shall submit a more specific emergency action plan for review prior to a building permit being granted. There were also four emails received which are in the packet.

J. Cornock – the Board could discuss potential revisions for the plan set with the applicant. No adjustments are currently proposed to the east array. There may be a walking path proposed along the pond for the south array.

J. Druley – the Board needs to discuss decommissioning.

M. Terry – the process typically followed with municipalities includes a decommissioning agreement to be approved as part of the overall Board decision approval. Town Counsel and Town staff can be worked with in terms of negotiating all associated documents. The decommissioning agreement and estimate will then come before the Board for its review. The applicant has a third party service provider to produce the estimates.

**There was agreement to continue this hearing to the February 10, 2026, meeting.**

*Applicant: Fashion Foods, LLC - Site Plan Review application for a proposed 2,819 square foot, 36-seat, Wendy's fast-food restaurant with drive-thru at 0 Worcester Ct (Map# 39 15 048 001 B).*

Bob Ament, representing the applicant – there was a meeting held this morning with the Town Engineers and Staff.

M. Tondera – the applicant has proposed mitigation measures for traffic which they will fund themselves. This will require Traffic Advisory Committee and the Engineering Department's approval and permits. These include changing the current stop signs along Spring Bars Road to solar powered LED flashing signs. On Worcester Court, solar powered speed radar signs will be installed. These will be conditions included in the decision. The applicant has proposed installing a stockade fence along the loading area of the east side of the property to mitigate any headlights coming into the property. In the area with parking within 5' of the property line, the parking has been set back 3' and each space will

have a steel bollard installed. Landscaping was proposed in the center island. A fence may be included along the corner of the landscape area behind the Wendys.

B. Ament – the previous plan showed a walkway from Spring Bars Road up through the site. The applicant was advised that the walkway must be handicapped accessible. This feature was thus eliminated. The 8' stockade fence installed along the lot line will keep headlights from affecting properties to the east. The mitigation proposed along Worcester Court should lower speeds approximately 3-5mph. The Town's consultant agrees that the project's impact on traffic operations will be minor. They suggested recommendations, including reconstructing the crosswalks at the intersections to be handicapped accessible. The applicant has agreed to pay the entire cost for this. Also, the walkway should meet handicapped compliance as well, which the applicant will adjust. The Engineering Department is adamant that the existing French drain not be utilized. Some changes to the plan in order to improve this are being worked on. Comments were made about the peer reviewer's report of the traffic analysis. The Town's consultant stated that the plans and traffic impact evaluation were done adequately. The current plan does not show a right turn only for customers on the access way. A 3' space along the south property line is being proposed for parking. The 5' setback could be complied with, but angled parking is important for the site in terms of one-way traffic flow. The Town Engineer stated that the flow for the site is good and the queuing is sufficient. The Town Engineer stated that the angled spaces need to be 20' deep and so angled parking at 3' from the line is a better plan. The current building design is more Cape Cod style and will be 26' to the ridge. There will be an uncovered patio, which reduces the lot coverage from 6.9% to 5.8%. 40% is allowed by right in this District. The consultant has stated that 21-29 spaces would be sufficient during peak hours and 39 are proposed. Additional professional information will be submitted regarding odors and noise.

Linda Clark, President of the Falmouth Housing Corporation – there have been great improvements made to the plan, but I still have concerns. The peer reviewer did not consider the driveway of Little Pond Place in relation to the Spring Bars Road exit. There are 32 vehicles that access the site and a driveway next door could be problematic. The peer reviewer also stated that he would look into the Plaza driveway across from the access way with Worcester Court. The drive-through speaker will likely be heard by abutters. 120-140 additional vehicles will travel through the area daily due to the proposed use. Trucks will be accessing the truck when the site is closed, which means early in the morning or late at night and this is a concern for noise. The elevation at the turn onto Spring Bars Road could be a concern in terms of the steepness.

T. Zine – I agree with the proposed angled parking. 18 parking spaces are proposed along the lot line, 12 of which will have bollards, and this could be a concern regarding the space between these lines especially when the lines are obscured. I would like to see additional bollards for safety between Wendys and the abutting building.

R. Lizardi – if more bollards are requested, a timber guardrail to create a continuous buffer would be proposed instead.

C. Harris – I believe a well-lit commercial enterprise in this area will help with safety concerns in terms of the existing vacant lot. I would like to know more about the proposed design in terms of making it more attractive.

The applicant team stated that there are different building designs in the Cape which could be used. The building is made tall with a flat roof in order to hide the mechanicals on the roof.

C. Harris – the landscaping plan has improved a lot.

M. Tondera – the Board received another comment email and a request from the applicant to extend the decision deadline to January 28<sup>th</sup>. A super majority will be needed for the Special Permit.

The Board indicated that the Special Permit application was also open at this time, for simultaneous discussion.

T. Zine – the comment letter received was not in support of the proposal.

J. Fox – I think bollards work well and leave extra space for people to move around.

A straw poll of the Board was in support of the bollards, as proposed.

B. Ament – the peer reviewer found that the 60' separation between the proposed driveway and that at Little Pond Place was adequate. The traffic engineer believes that these driveways will function properly. Any development on this site will lead to additional traffic.

M. Tondera reviewed the proposed additional conditions regarding traffic mitigation items.

The Board agreed that a positive motion could be drafted for the next meeting.

**MOTION** by j. Fox/C. Harris to continue the Special Permit application to the January 27, 2026, meeting.  
**Voted 6-0-0.**

**MOTION** by R. Leary/C. Harris to extend the decision deadline for the Site Plan Review.  
**Voted 6-0-0.**

**PUBLIC HEARING:**

*Applicant: Fashion Foods, LLC - Special Permit application to allow a portion of the restaurant parking to be located in the front yard, and for 18 parking spaces on the south side of the lot to be within 5 feet of the property line at 0 Worcester Ct (Map# 39 15 048 001 B).*

*This item was addressed concurrently with the previous agenda item.*

**PLANNING BOARD DISUCSSION**

- "Cape Cod Commission's Vote Led to the Lyberty Green Fiasco"- Eric Turkington's Falmouth Enterprise Column

J. Druley – a subcommittee for the Cape Cod Commission supported the hotel. The full Planning Board voted not to support the hotel. The Select Board wrote a letter to the Commission supporting the hotel.

- Planning Department Report

M. Tondera - the Select Board voted to add an Article to April Town Meeting to support a Seasonal Communities designation. The Board needs to discuss zoning changes for the November Town Meeting.

J. Cornock – if Town Meeting approves the Seasonal Communities designation, the Board will have to consider regulations allowing tiny homes by-right and the ability to develop undersized lots. The Planning Department’s Office will be moved next to the Zoning Board of Appeals Office.

- Committee Reports

C. Harris – the Short-term Rental Advisory Group sent its recommendation to the Select Board, who is considering it for the April Town Meeting.

P. Dreyer – the Water Resources Committee agreed to draft a letter to the Select Board, recommending that a subcommittee be formed, consisting of members of the Finance Committee, Select Board, and others to discuss a fair and simple financing plan for water projects.

**ANNOUNCEMENTS:** None at this time.

**GENERAL CORRESPONDENCE:** December 31, 2025, through January 13, 2026: As discussed.

**FUTURE AGENDA ITEMS:**

**NEXT MEETINGS:** January 27, 2026; February 10, 2026

The meeting was adjourned at 9:31pm.

**Voted 6-0-0.**

Respectfully Submitted,  
Kristan Patenaude, Recording Secretary  
Tom Zine, Clerk



December 23, 2025

Jed Cornock, AICO, Town Planner  
Falmouth Planning Department,  
Town Hall Square  
Falmouth, MA 02540

**RE: Site Plan Review & Special Permit applications**  
**Applicant: Fashion Foods, LLC**  
**Lot 1B, 0 Worcester Court, Falmouth**  
**Assessors ID: 39 15 048 001B**

Dear Jed:

We appreciate you meeting with Bob Ament and myself to review the above reference applications. Please accept this letter, and enclosed revised documents, in response to the discussion at the Planning Board public meeting and public hearing held on December 9<sup>th</sup>, 2025 and to address items from our meeting with you. This letter, and enclosed documents, also serves as response to the follow up comments submitted by the Falmouth DPW Engineering Division to the Planning Board Referral request, dated 12/9/2025 and submitted by Scott Schluter, P.E., Staff Engineer.

Most of the items noted in the Falmouth DPW follow up referral were discussed at the December 9th meeting with the Planning Board. But certain items are further discussed here to identify and substantiate the reasons supporting the design as presented. We request the Planning Board to please review the explanation provided here for certain aspects of this project during deliberation.

First, the Site Plan has been revised as follows:

1. Provide construction installation details for the proposed signage, fencing and bollards. The signage details illustrate the typical regulatory and warning traffic signs, site specific traffic signs, and parking signs.
2. Revise parking striping to increase parking stall lengths for the 45-degree parking spaces north of the drive-thru aisle. This revision did not change the site layout design but rather lengthen the parking painted line stripes by one-foot and reduced the driveway width by one-foot. We do not think that Bylaw Section 240-14.1G(1) requires this change, but the Engineering Department requested it.
3. Revise parking layout to provide a 3-foot parking setback along the southern property line (an increase of one-foot) and to introduce 12 steel bollards as guards from vehicles driving through the property line. This design does not compromise the width of the driveways previously proposed. The proposed bollards provide better protection to the abutter's

structure from potential vehicle impacts, a concern expressed by the abutter representative at the last meeting. This design maintains the preferred parking layout compared to a parking layout that provides a 5-foot setback to the property line. Although it is possible to design the site with the 5-foot parking setback by providing 90-degree parking spaces, the actual use of the site is affected if vehicles attempt to exit their parking spaces against the one-way traffic flow. The proposed bollards with a 3-foot parking setback are a good compromise to provide a better parking layout design while protecting the abutter's structure.

4. Add a stockade fence, for 64-feet, from the rear left side corner of the dumpster enclosure fence and along the property line to provide immediate screening to the easterly abutting residences. The row of arborvitae that was proposed would not have established a comparable screen for a few years. Aside from being an immediate screen from car headlights, the fence will block debris, a concern raised by the abutter's representative, from passing through this area. This added fence eliminates the previously proposed arborvitae screen which was criticized by the abutter and not effectively providing adequate screening.
5. Eliminate the easterly walkway from Spring Bars Road due to complications regarding ADA accessibility. After an attempt to incorporate this walkway into the design, the Architectural Access Board of Massachusetts confirmed that such a walkway would need to comply with accessibility requirements. These requirements would introduce a ramp system in excess of 52-feet given the four-foot vertical grade difference from the existing Spring Bars Road public sidewalk, at elevation 16-feet, to the grades along the proposed driveway and loading zone at elevation 20-feet and higher. The amount of grading that this would require, and the additional length that a ramp system would introduce, will be very difficult. The ramp system is also impractical as pedestrians will likely avoid walking through a back-and-forth ramp system when an existing pedestrian route already exists that provides an accessible route to the entrance. The existing sidewalk route is hardly longer than would be such a ramped walkway from Spring Bars Road. The applicant therefore asks the Board to not require a pedestrian walkway from Spring Bars Road.

Aside from the revisions to the Site Plan noted above, other materials for this application have been revised and updated, and new documents are being provided as follow:

6. The landscape plan has been revised for consistency with the latest Site Layout and to incorporate changes to the landscape design.
7. Additional landscape beds are proposed within the "green" space between Wendy's and the drive-thru queue aisle.
8. A Stormwater Management System Operation & Maintenance (O&M) Plan is provided. The O&M Plan informs the Planning Board of who is responsible for inspection and maintenance of the drainage system. The document also informs the necessary inspection

tasks for the different stormwater management components and corrective actions recommended should any components requires additional attention, cleaning or replacement. Included in the O&M Plan is maintenance of the proposed trench drain which is only proposed to intercept surface runoff from approximately 370 square feet of the exit only paved driveway. Three other proposed catch basins control the vast majority of runoff.

9. Although the sight distance triangle graphics submitted illustrate that drivers have sufficient unobstructed sight distance from the two driveways, these graphics have been revised to depict the 335-foot triangle sight distance as requested by the Town Engineer. This revision does not alter in any way the proposed site Layout or the proposed driveways.
10. A new graphic plan is provided to illustrate the recommended location for “Speed Radars” along Worcester Court. The recommended location of the “Speed Radars” are 475-foot north and 450-foot south from the Worcester Court – Spring Bars Road intersection. These two locations do not appear to interfere with existing roadways or driveways and appear to be in areas that receive enough sunlight to adequately power the system (solar cell energy).

We believe the supplemental information provided here addresses all open items from the public meeting and hearing and respectfully ask for your support for this application.

Please contact me at 508.477.7272 or raul@capeeng.com if there are any comments of questions.

Sincerely,



Raul Lizardi-Rivera, P.E.

*Director of Engineering*

Cape & Islands Engineering, Inc.

800 Falmouth Road Suit 301C

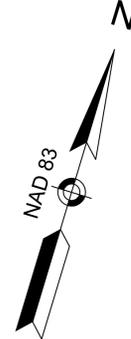
Mashpee, MA 02649

(508) 477-7272

enclosed: Site Plan revision date 12/22/2025  
Landscape Plan revision date 12/22/2025  
Graphic Plan – Recommended Speed Radar Signs  
Revised Graphic – Main Drive Sight Distance revision date 12/18/25  
Stormwater Management System O&M Plan



LOCUS MAP NOT TO SCALE



RIVER ROCK PLANTING				
QUAN.	SIZE	BOTANICAL NAME	COMMON NAME	NOTES
6	5 GAL.	ILEX CORNUTA (DWARF)	DWARF HOLLY	
3	5 GAL.	JUNIPERUS TOUROLOSA	TWISTED JUNIPER	
35	1 GAL.	IRIS RETICULA	NETTED IRIS	
9	5 GAL.	JUNIPERUS HORIZONTALIS	BLUE RUG JUNIPER	
25	1 GAL.	AMMOPHILA BREVILIGULATA	BEACH GRASS	
1	5 GAL.	PICEA GLAUCA "CONICA"	DWARF ALBERTA SPRUCE	
4	5 GAL.	CORNUS FLORIDA	CHEROKEE CHIEF DOGWOOD	
HEDGE	--	BUXUS MICROPHYLLA	BREEN BEAUTY BOXWOOD	

SITE NUMBER: TBD  
 BUILDING TYPE: GNG 40  
 ASSET TYPE: FREE STANDING  
 CLASSIFICATION: NEW  
 OWNER: FRANCHISE  
 BASE VERSION: 2025  
 UPGRADE CLASSIFICATION: NEW BUILD  
 PROJECT YEAR: 2026  
 DESIGN TYPE:  
 DRAWING RELEASE: SUMMER 2025

**UPLAND ARCHITECTS**  
 250 E. MAIN STREET - SUITE 13  
 NORTON, MASSACHUSETTS 02766  
 331 SOUTH WASHINGTON STREET  
 SHELBY, NORTH CAROLINA 28150  
 T 774-430-3390  
 WWW.UPLANDARCHITECTS.COM

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PROJECT TYPE: GNG 40

**Wendy's**  
 FASHION FOODS LLC  
 LOT 1B WORCESTER COURT  
 FALMOUTH, MA 02540

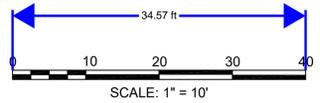
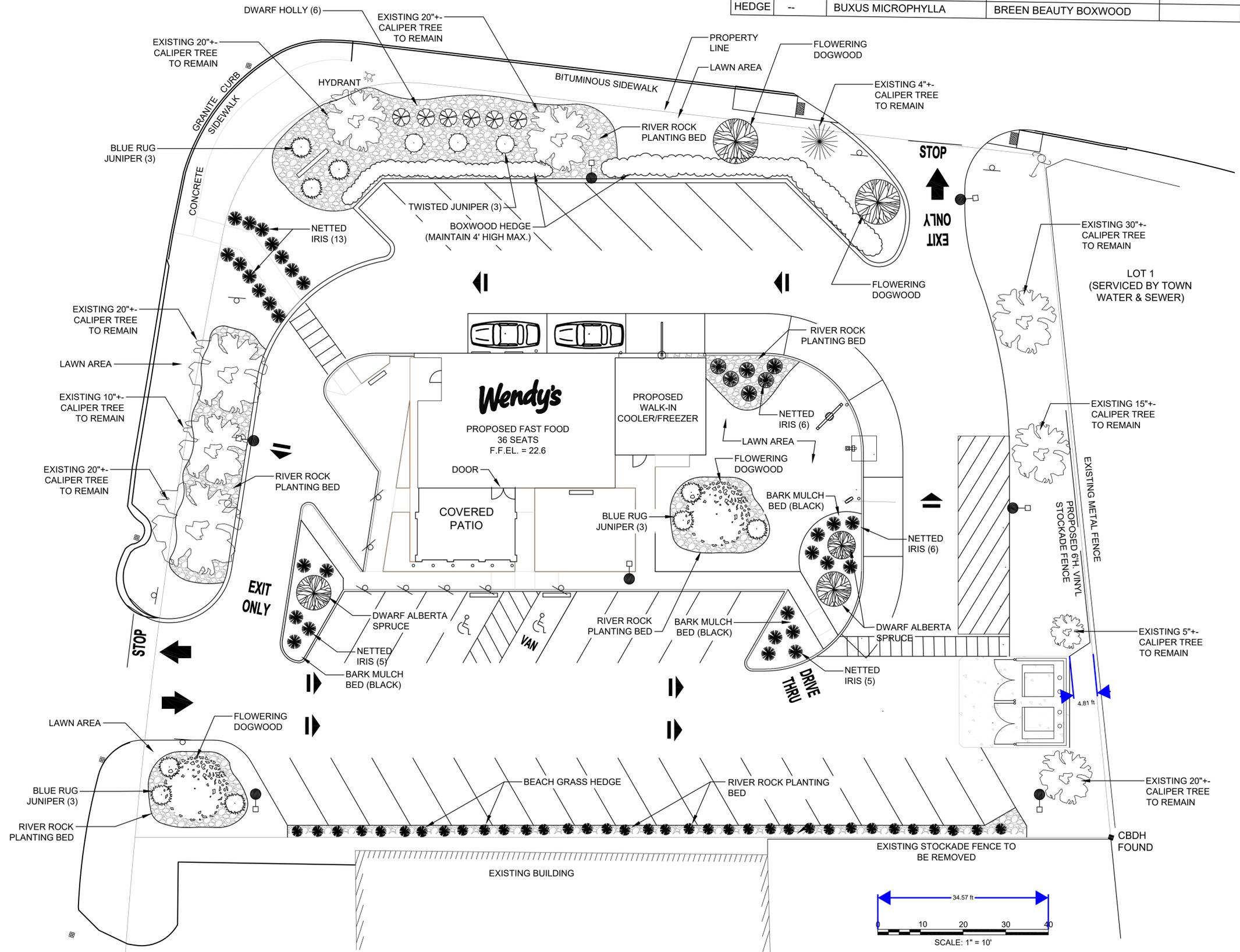
REV.	DATE	DESCRIPTION

ISSUE DATE: 12/22/2025  
 PROJECT NUMBER: 25-01-013  
 DRAWN BY:  
 CHECKED BY:



SHEET NAME  
**SCHEMATIC LANDSCAPE PLAN**  
 SHEET NUMBER

**LP-100**  
 (Updated 12-22-25)



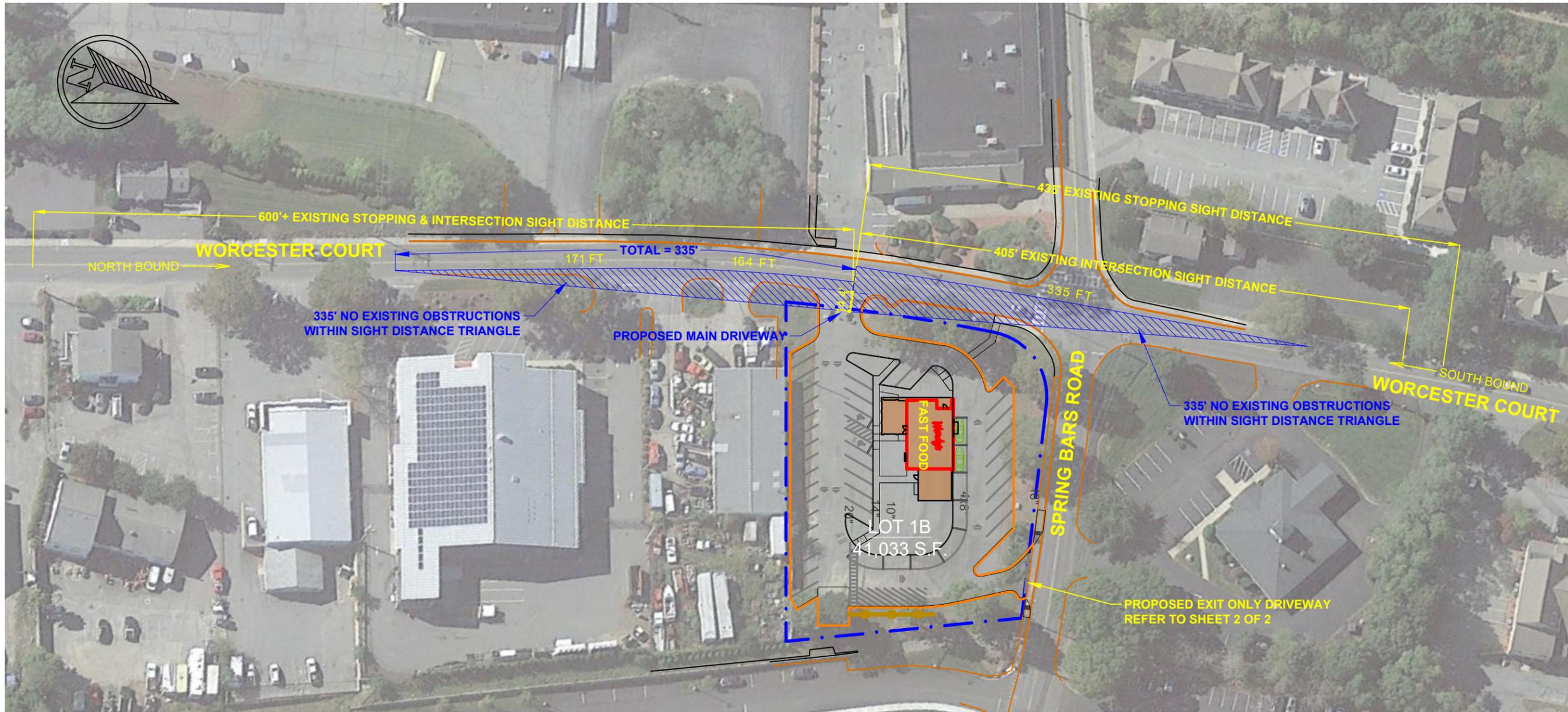


IMAGE NOTE  
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THE MEASURED (EXISTING) SIGHT DISTANCE INFORMATION REPRESENTED IN THIS GRAPHIC AND TABLE WAS OBTAINED FROM THE VHB TRAFFIC REPORT.  
THE FALMOUTH DPW - ENG. SIGHT DISTANCE INFORMATION REPRESENTED IN THIS GRAPHIC AND TABLE IS BASED ON DISTANCES FROM THE TOWN OF FALMOUTH DPW - ENGINEERING DIVISION "CLEAR SIGHT TRIANGLE EXAMPLES PURSUANT TO 199-3 DIMENSIONS OF OBSTRUCTED VIEW" REVISION 1 - 8/27/21.

### SIGHT DISTANCE ANALYSIS SUMMARY

LOCATION	STOPPING SIGHT DISTANCE (FT.)			INTERSECTION SIGHT DISTANCE (FT.)		
	TRAVELING	FALMOUTH DPW-ENG.	MEASURED	TRAVELING	FALMOUTH DPW-ENG.	MEASURED
WORCESTER COURT AT SITE DRIVEWAY	NORTHBOUND	335	600+	LEFT	335	600+
	SOUTHBOUND	335	440	RIGHT	335	410

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Prepared For: FASHION FOODS LLC

Project: WENDY'S  
LOT 1B WORCESTER COURT  
FALMOUTH, MA 02540

Drawing Title: GRAPHIC - MAIN DRIVEWAY  
SIGHT DISTANCE

Drawn By: RLR Checked: RLR

Sheet: 1 OF 2 Date: 9/23/2025

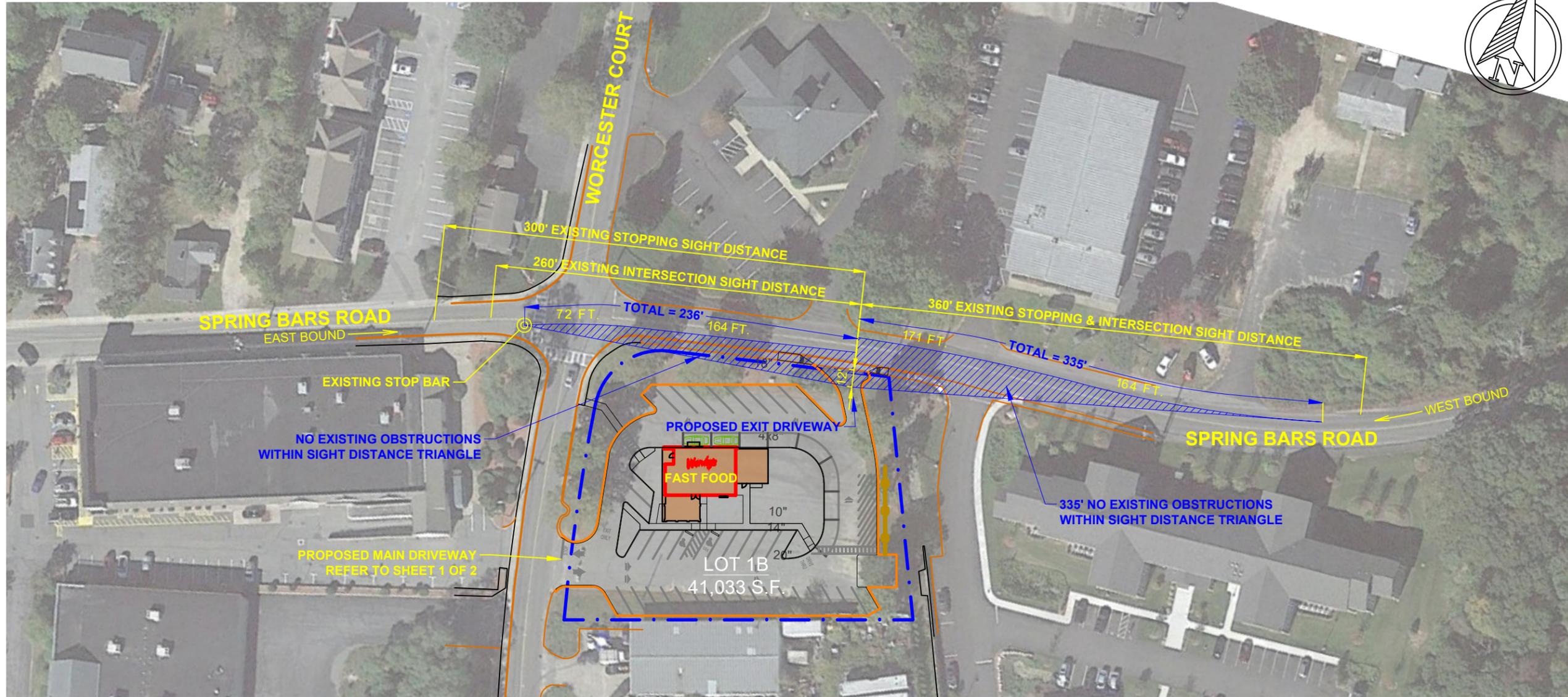


IMAGE NOTE  
THE AERIAL IMAGE USED IN THIS GRAPHIC WAS OBTAINED FROM PUBLICLY AVAILABLE GOOGLE EARTH MAPS IMAGES.

THE MEASURED (EXISTING) SIGHT DISTANCE INFORMATION REPRESENTED IN THIS GRAPHIC AND TABLE WAS OBTAINED FROM THE VHB TRAFFIC REPORT.  
THE FALMOUTH DPW - ENG. SIGHT DISTANCE INFORMATION REPRESENTED IN THIS GRAPHIC AND TABLE IS BASED ON DISTANCES FROM THE TOWN OF FALMOUTH DPW - ENGINEERING DIVISION "CLEAR SIGHT TRIANGLE EXAMPLES PURSUANT TO 199-3 DIMENSIONS OF OBSTRUCTED VIEW" REVISION 1 - 8/27/21.

### SIGHT DISTANCE ANALYSIS SUMMARY

LOCATION	STOPPING SIGHT DISTANCE (FT.)			INTERSECTION SIGHT DISTANCE (FT.)		
	TRAVELING	FALMOUTH DPW-ENG.	MEASURED	TRAVELING	FALMOUTH DPW-ENG.	MEASURED
SPRING BARS ROAD AT SITE DRIVEWAY <sup>1</sup>	EASTBOUND <sup>2</sup>	236	260	LEFT	236	300
	WESTBOUND	335	360	RIGHT	335	360

- There are no posted speed limit signs along Spring Bars Road in the vicinity of the site. A speed of 25 mph eastbound and 30 mph westbound were assumed which is likely conservative as there are stop signs on Spring Bars Road immediately to the west of the site driveway.
- The measured (existing) distance is limited at the intersection of Spring Bars Road by a vertical curve in the road. However, the westbound traffic stop sign at the Worcester Court intersection significantly slows the approaching traffic speed.

REV. 12/18/25: REVISE BOTH SIGHT DISTANCE TRIANGLES TO ONLY 335-FOOT LENGTH ALONG TRAVELED LANE

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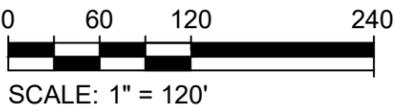
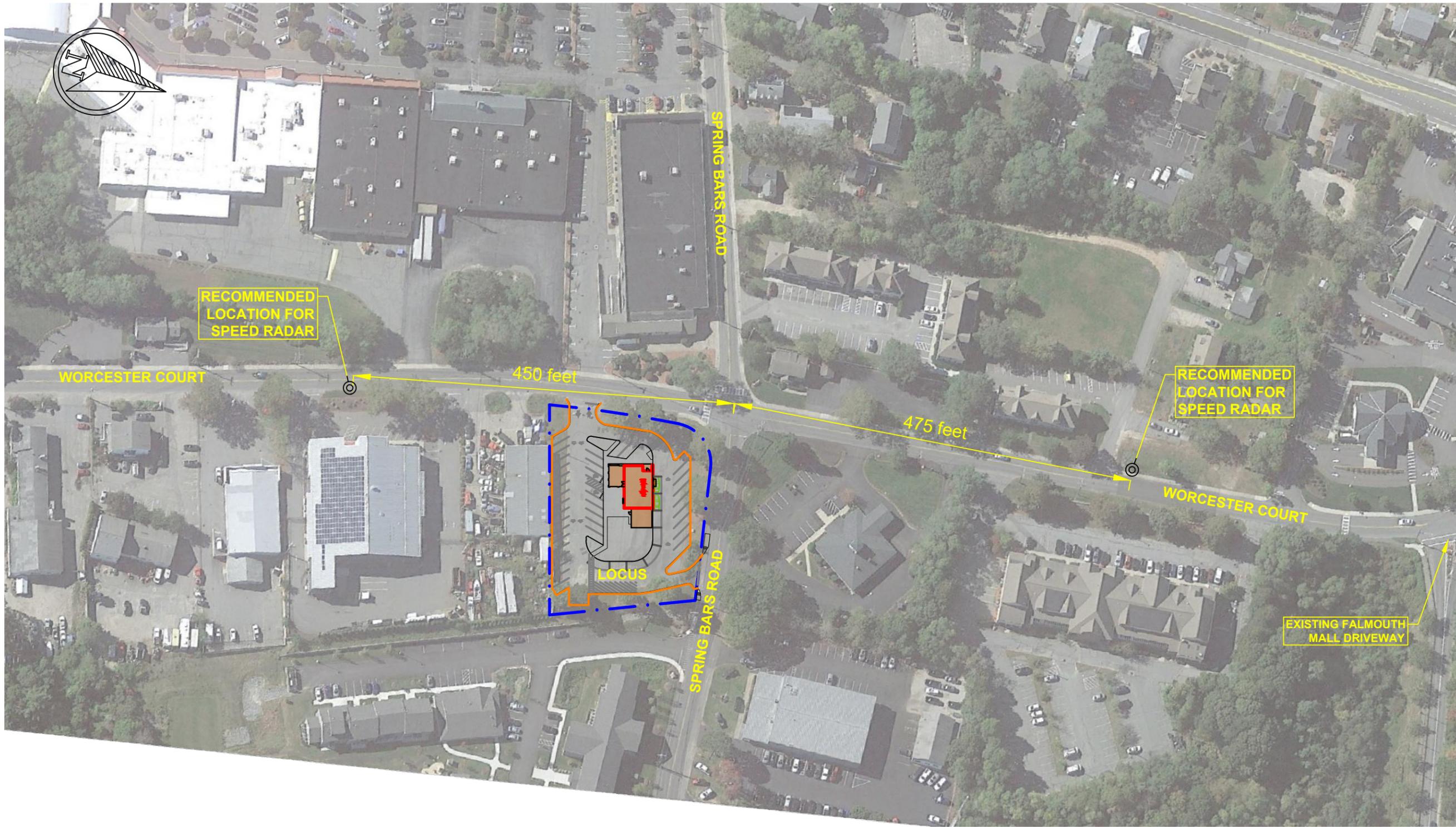
Prepared For:  
**FASHION FOODS LLC**

Project:  
**WENDY'S  
LOT 1B WORCESTER COURT  
FALMOUTH, MA 02540**

Drawing Title:  
**GRAPHIC - EXIT ONLY DRIVEWAY  
SIGHT DISTANCE**

Drawn By: RLR      Checked: RLR

Sheet: 2 OF 2      Date: 9/23/2025



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Prepared For:	FASHION FOODS LLC	
Project:	WENDY'S LOT 1B WORCESTER COURT FALMOUTH, MA 02540	
Drawing Title:	GRAPHIC PLAN RECOMMENDED SPEED RADAR SIGNS	
Drawn By:	RLR	Checked: RLR
Sheet:	1 OF 1	Date: 12/18/2025



Outlook

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**[EXTERNAL] - Fashion Foods, LLC, Site Plan Review, proposed Wendy's at 0 Worcester Court**

---

From Bob Ament <bob@amentklauer.com>

Date Wed 1/7/2026 11:15 AM

To Melinda Tondera <melinda.tondera@falmouthma.gov>; Jed Cornock <jed.cornock@falmouthma.gov>

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Melinda:

Fashion Foods, LLC. is agreeable to extend the deadline for the Planning Board to file its Site Plan Review decision in the above matter until Wednesday, January 28, 2026.

Yours,

Bob Ament

Robert H. Ament

***Ament Klauer LLP***

39 Town Hall Square

Falmouth, MA 02540

Phone: (508) 540-6555

Fax: (508) 457-1293

Email: [bob@amentklauer.com](mailto:bob@amentklauer.com)

Website: [www.amentklauer.com](http://www.amentklauer.com)

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# Town of Falmouth

Department of Public Works - Engineering Division

416 Gifford Street, Falmouth, MA 02540  
Office: 508-457-2543, Fax: 508-548-1537

Jim McLoughlin, P.E., Town Engineer

James.McLoughlin@falmouthma.gov

## Combined Review – Planning Board – Site Plan Review, Special Permit Zoning Board of Appeals – Special Permit Application 071-25

To: Jed Cornock, AICP, Town Planner  
Noreen Stockman, Zoning Administrator  
CC: File

From: Scott Schluter, P.E., Staff Engineer

Date: 1/7/2026

Re: 0 Worcester Court, Assessor Parcel 39 15 048 001B

---

The Engineering department received the following to review for the above-referenced project:

- Letter – Re: Site Plan Review & Special Permit Applications, 3 pages, to Jed Cornock, Town Planner, from Raul Lizardi-Rivera, P.E. of Cape & Islands Engineering, dated December 23, 2025.
- Civil Site Plan Set - “Wendy’s,” by Cape & Islands Engineering, dated August 6, 2025, last revised 12/22/25, 4 Sheets.
- Landscape Plan – “Schematic Landscape Plan,” by Upland Architects, dated 12/22/2025, with no revisions, sheet LP-100.
- Sketch Set – “Graphic – Main Driveway Sight Distance,” and “Graphic – Exit Only Sight Distance,” by Cape & Islands Engineering, dated 9/23/2025, last revised 12/18/25.
- Sketch – “Graphic Plan, Recommended Speed Radar Signs,” 1 sheet, by Cape & Islands Engineering, dated 12/18/2025, with no revisions.
- Document – “Lot O, Worcester Court – Wendy’s Restaurant, Stormwater Management System Operation and Maintenance Plan,” 2 pages, signed by Josh Reine, CFO, Fashion Food, LLC, dated 12/19/2025.

Below is a summary of our original comments and responses, sorted for clarity. Original comments are in *italics*, current comments are in **bold**. Previous pages omitted where not necessary.

**Addressed Comments** are closed topics for DPW Engineering. Previously addressed comments were omitted.

**Requested Conditions** are topics that can be addressed with a Condition.

**Deferred to Others** are topics that are not within DPW Engineering’s jurisdiction, and other Departments should determine if each of these comments should be addressed.

**Open Comments** are topics that remain open for DPW Engineering.

**New Comments** are topics new to the revised information submitted.

---

## Addressed Comments

---

### Parking and Access

4. Worcester Court and Spring Bars Road are Public right-of-ways in this area. The proposed alterations to the right-of-ways would require filing a permit with the Engineering Division. What is shown on the plans would not be approved.

o The sight distance sketches do not meet the Town's standards, see:

<https://www.falmouthma.gov/DocumentCenter/View/53/Driveway-Sight-Distance-Detail-PDF?bidId=>

- Worcester Ct is posted 30 MPH; 335-feet would be required in both directions, and Spring Bars Road is 30 MPH by default; 335-feet would also be required in both directions. Note that the Worcester Ct and Spring Bars Road intersection would be the limit of the sight distance triangles in those directions.
- Sight distance is measured along the approach vehicle's travelled path.
- The Spring Bars Road clear triangle should take into account the elevations of the ground within the triangle.

The Boards should decide if the site plans should be revised.

DPW Engineering will require updated sight distance sketches in the road permit application; Planning and Appeals should determine if these sketches should be submitted for their approvals.

*We believe this site can provide the sight distance we typically require as part of a street permit. As stated in our review, the measurement is the travelled path of the moving vehicle, see below example for non-straight road sections.*

*We will require sketches that show the clear triangles for 335-feet along the travelled path with the exception of the triangle towards the Spring Bars Road and Worcester Court intersection; the length of which is fixed to the car stopped at this intersection.*

*The Applicant's Engineer has provided an explanation of how they feel they meet the requirements for the current applications.*

*The Boards should decide if they want to:*

- Accept the sight distance sketches submitted;*
- Require the Applicant to submit the sight distance figures that the DPW Engineering Division will require for the street permit before closing the hearing; or*
- Require the Applicant to submit to Planning the sight distance figures approved by DPW Engineering under an approved street permit.*

**Comment addressed, the Applicant's Engineer has submitted sight distance sketches showing compliance as requested. We would accept and approve these sketches if submitted with a Street Opening Permit application.**

14. The parking spaces do not appear to meet the required dimensional standards. The 18-foot stall length must reflect the usable parking depth. In perpendicular configurations, the stall length and line length are typically the same. However, for angled parking, the line length must exceed the stall length to ensure the required usable depth is achieved.

As illustrated below, the longer length allows a vehicle to park in one of the angled spaces without extending into the drive aisle. (The figure is for 60-degree parking with 9x18 stall dimensions.)

Note that at 9x18 feet, the Falmouth Standard size is already less than the MassDOT light-duty pickup truck control vehicle length of 20.3 feet.

Note that for 45-degree parking, the stall length is increased to 19-feet.

The Boards should decide if the parking lot layout should be revised.

*The plans were revised:*

- The 60-degree spaces provide the 18' stall length.*
- The 45-degree spaces do not provide the 19' stall length. (Note that the proposed drive aisle is wider than required at 15-feet; 12-foot wide is required.)*
- The one-way drive aisle width is met.*

*The Boards should determine if the proposed parking and drive aisle layout is acceptable.*

**Comment addressed. The plans were revised and the layout is compliant.**

---

## Requested Conditions

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### Parking and Access

8. We request that the Boards either require the Applicant to obtain an approved permit from the Engineering Division before closing the hearings, or that the Boards include the following condition in any approval:  
“The Applicant shall obtain an approved street permit and post any required bond with the Engineering Division before the start of construction. The Applicant shall complete the work approved by the Engineering Division in the permit.”

**No change in status, please include this condition in any approval.**

18. An address assignment will be required for construction. Based on current policy, the address assigned when a Building Permit is applied for would be:

189 Spring Bars Road

(A different odd number around 189 can be assigned if requested)

We recommend the following condition be included (or similar) in any approval:

“Upon completion of construction, the Applicant shall post the address for this business per §99-1 Affixing of legible numbers required; time limit for compliance.”

“The Applicant shall contact the Engineering Division for address assignment before issuance of a Building Permit.”

**No change in status, please include this condition in any approval.**

### Grading and Drainage

25. Erosion controls are shown on the plan and appear to be adequate to protect abutting properties and the public right of way.

We recommend the following condition be included (or similar) in any approval:

“Construction of this project shall follow the Town’s ‘Soil Erosion & Sediment Control Standard Conditions’ document, which can be found on the Engineering Division’s page of the Town’s website.

The Department of Public Works Engineering Division will oversee the erosion controls as part of the street permit.

**No change in status, please include this condition in any approval.**

---

## Deferred to Others

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### Parking and Access

7. Both entrances exceed the maximum opening width in §240-14.11(4)b. The turning movement shown for the WB62 vehicle does show that additional width is required for this vehicle to access the site.

The Boards should determine if the Applicant has demonstrated that the greater width is needed.

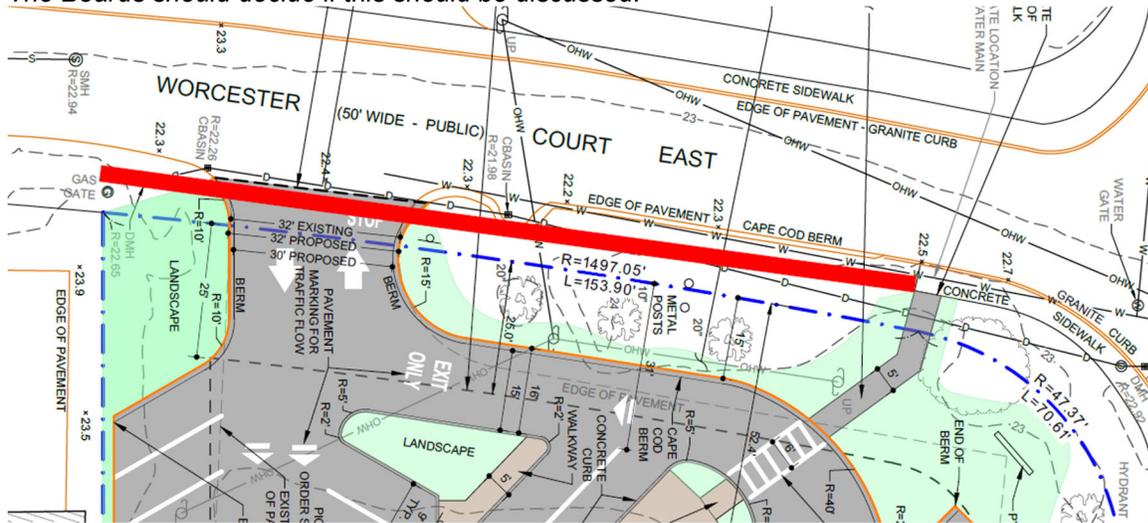
*The Applicant’s Engineer indicates that the proposed driveway widths are required. We would not have an issue approving the additional width under a street permit.*

*The Boards should decide if the plans should be revised to meet the Zoning requirements, or if the Applicant’s Engineer has demonstrated the need for relief as allowed by §240-14.11(4)b.*

**No change in status.**

11. Has the Applicant considered extending the sidewalk from the current termination point on Worcester Court across the full frontage of the property?  
The Boards should decide if this should be discussed.

The Boards should decide if this should be discussed.



The Boards should decide if there would be a benefit to extending the sidewalk across the entire frontage of Worcester Court as shown below in red, or if the plans should be approved as submitted.

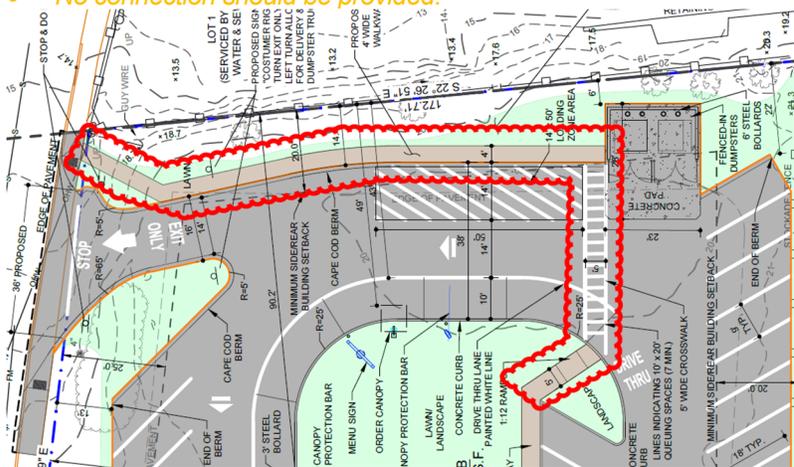
**No change in status. We recommend that sidewalks be extended across all frontage of a commercial property.**

12. Has the Applicant considered providing pedestrian access from Spring Bars Road into the site?  
The Boards should decide if this should be discussed.

The plans were revised to include a sidewalk connection to the existing sidewalk in Spring Bars Road, however, the Applicant's Engineer offers reasons for not including this in the final design due to safety concerns. If a connection along Spring Bars Road is not included in the final design, pedestrians walking west on Spring Bars Road can access the site by walking up to Worcester Court to the sidewalk connection to the front door.

The Boards should decide if:

- The pedestrian connection to Spring Bars Road should remain as currently shown (see revision cloud area below);
- Alternate routes should be explored; or
- No connection should be provided.



The revised plans do not include the sidewalk. The Applicant's Engineer indicates that the Architectural Access Board of Massachusetts has opined that this sidewalk would be required to meet the slope limitations for ADA accessibility.

**The Board should decide if:**

- **The previously proposed pedestrian connection to Spring Bars Road should be revised to meet ADA compliance, or**
- **The proposed alternate route of continuing up Spring Bars Road to the walkway at the intersection with Worcester Court is acceptable.**

13. A landscape plan was included. It isn't clear if the parking lot and service area screening, vegetated islands for parking lot interiors, and residential buffer requirements are met.

The Boards should decide if the performance standards are met by the submitted plans.

The plans were revised to include additional screening with vegetation and fencing. The Boards should review the revised plans.

**No change in status. The site landscaping was revised; the Boards should re-evaluate.**

**Grading and Drainage**

19. Is there any soil testing data available to confirm the materials and justify the percolation rate used for stormwater system design?

The Boards should determine if soil exploration data should be provided.

The Applicant's Engineer indicates that there is not exploratory test data for this site; however, there is available information for nearby lots that indicates that the underlying soils are likely sandy.

The Boards should decide if:

- Soil testing is required before closing the hearing;
- There should be a Condition to submit soils information for review before installation of the stormwater infiltration areas; or
- The additional information provided is evidence enough that the system would be installed in sandy soils.

**No change in status. The Boards should decide if, and if required, when soil information should be provided.**

20. The submitted stormwater calculations do not conform to the Town's standards. We have previously provided similar comments regarding the calculation template used in this submission, as it has also been employed in other projects. We recommend that the Applicant's Engineer revise the template to align with the Town's current stormwater requirements before resubmitting for this and future projects.

o Inclusion of infiltration through the system's bottom area is typically not allowed.

o No backup information is provided for some calculations; the equations used in the cells should be included in the template. For example, the Rational Method equation was used to determine the flow rate, but this is not shown anywhere.

o It's not clear why HydroCAD calculations were also included. Information that doesn't support showing compliance with the Town's Standards should not be included in submissions. Note that the storage volumes in this model do not match what was used in the calculations template.

The Boards should decide if revised calculations should be submitted.

The Applicant's Engineer provides arguments regarding applicable Zoning regulations and the previously submitted calculations, and that there are no regulatory requirements for which methodology is used for calculations. As noted for a recent Site Plan Review, these standards are what DPW Engineering has held for 15+ years for Planning Reviews.

The Boards should determine if:

- The calculations should be revised to conform to the typically held standards, or
- The calculations comply with the regulations and are acceptable as submitted.

**No change in status. The Boards should decide if the calculations, as submitted, are acceptable.**

22. The drainage area map indicates that not all areas of the site would be captured and infiltrated onsite, which is a Town Standard that the Boards typically hold (§240-13.1C(2)b.). This site has some challenging slopes along some of the perimeter, and the areas without stormwater mitigation are landscaping, lawn, and areas that won't be disturbed (see areas in red below that have no stormwater mitigation).

The Boards should determine if the stormwater mitigation plans for the site are acceptable.

*The Applicant's Engineer requests that the submitted stormwater mitigation plan for the site be accepted due to a significant reduction in stormwater runoff.*

*The Boards should determine if the stormwater mitigation plan, as submitted, is acceptable.*

**No change in status. The Boards should decide if the calculations, as submitted, are acceptable.**

24. Were the stormwater structures along the frontages of this site opened and examined?

o It should be confirmed that no pipes enter the site from any of these structures.

o It should be determined what elevation the drainage pipe is at where the water connection is proposed.

o Town GIS shows an additional pipe in the direction of Spring Bars Road at this catchbasin; it should be determined if this pipe exists.

We recommend that additional information about the stormwater around the site be added to the plans.

*The Applicant's Engineer provided additional information from the Little Pond Service Area sewer project, where the existing conditions surveyor couldn't obtain inverts for the stormwater appurtenances in the Town right of way. Our main concern would be an unmapped stormwater pipe into the site. The stormwater pipe elevation at the water crossing can be determined at the installation of the water service. The additional pipe shown on GIS was added to the plan.*

*The Boards should determine if the plans are acceptable as submitted, or if invert information should be added to the plans.*

**No change in status. The Boards should decide if the plans are acceptable as submitted, or if additional information should be added.**

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### Open Comments

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#### Parking and Access

4. Worcester Court and Spring Bars Road are Public right-of-ways in this area. The proposed alterations to the right-of-ways would require filing a permit with the Engineering Division. What is shown on the plans would not be approved.

o The proposed stormwater management for the Spring Bars Road driveway does not appear to meet the requirement that no stormwater from a driveway can discharge into a public right-of-way.

*The plans were revised to include a trench drain to capture all stormwater from the driveway; we do not allow the use of trench drains to prevent stormwater runoff from entering public right-of-ways.*

**No change in status. The Town Engineer has confirmed that a Street Opening Permit that includes a trench drain would not be approved, notwithstanding any submitted O&M materials.**

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### New Comments

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A. A site signage plan was submitted.

**Where the signs are onsite directional or informational signs, we have no comment.**

- B. The site layout was revised to provide a 3-foot setback from the southern abutter's property line. Bollards are proposed between the parking spaces and the property line to prevent encroachment onto the abutting property.

The Applicant's Engineer indicates that the required 5-foot setback can be provided with layout modifications, but the current proposed layout is desired.

**The Boards should decide if the required setback should be provided, or if the plans are acceptable as submitted.**

- C. The proposed arborvitae screening was removed from the plans and replaced with a stockade fence to screen headlights from the site onto the abutting property to the east.

**The Boards should decide if the revised proposed screening is acceptable.**

- D. Additional landscaping was added near the drive-thru area.

**The Boards should review the new landscape plan.**

- E. An Operation and Maintenance Plan was submitted for the on-site stormwater management system.

**The Boards should decide if this O&M should be referenced in an approval.**

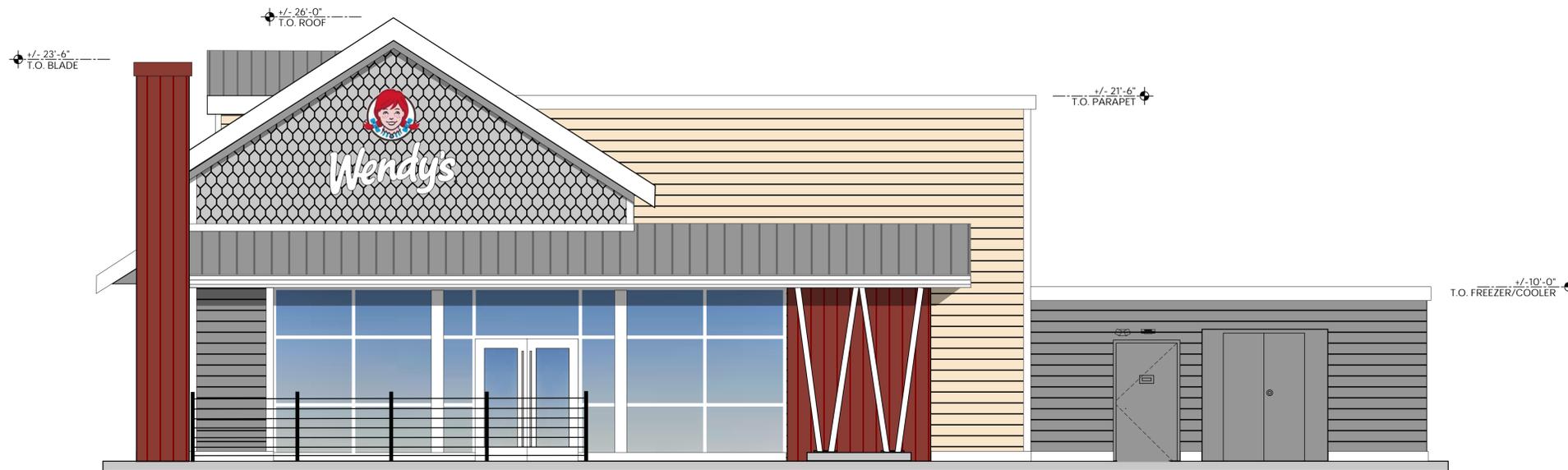
- F. A sketch showing where two "speed radars" should be located was submitted. It is not clear what the intent of this sketch is.
- a. Will the Applicant be purchasing and installing these?
  - b. Installation would require approval from the Traffic Advisory Committee. While similar installation locations have been approved in the past, not all have received Town funding and therefore not all have been installed.
  - c. Installation by a private contractor would require a street permit from the Department of Public Works Engineering Division.

**If the Boards intend to base their approvals of this project on the installation of the proposed "speed radars," the Board should determine whether:**

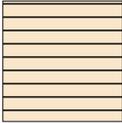
- **The installation should be approved and funded — whether privately by the Applicant or by the Town — before any project approval, or**
- **A condition should be included in the approval requiring that the devices be approved and installed. If the installation is not approved, or if it is approved but not funded, the Applicant must return to the Boards to request a modification of the approval.**

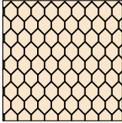
-End

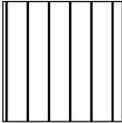


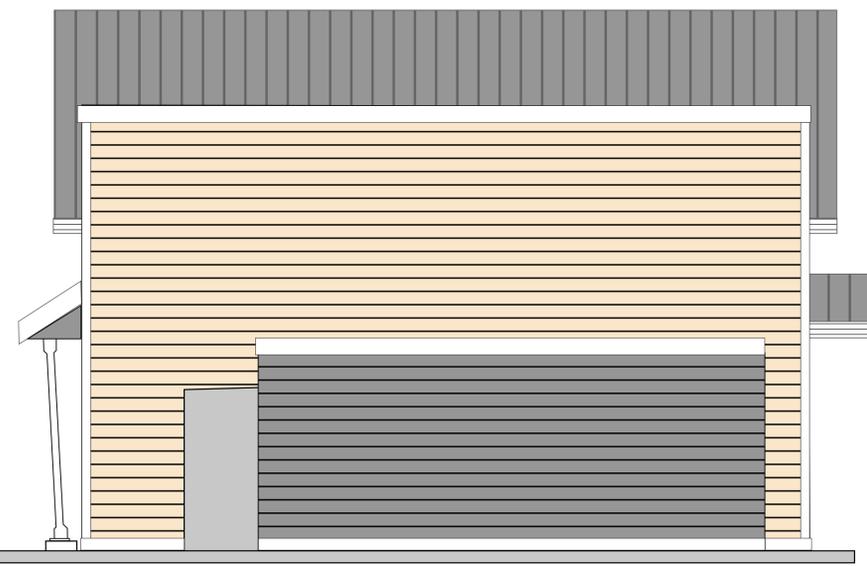


 silver standing seam roof

 color TBD hardie board/siding

 color TBD cedar shake

 color TBD board & batten/siding



SITE NUMBER:	TBD
BUILDING TYPE:	GNG 40
ASSET TYPE:	FREE STANDING
CLASSIFICATION:	NEW
OWNER:	FRANCHISE
BASE VERSION:	2025
UPGRADE CLASSIFICATION:	NEW BUILD
PROJECT YEAR:	2026
DESIGN TYPE:	
DRAWING RELEASE:	SUMMER 2025

**UPLAND ARCHITECTS**  
 250 E. MAIN STREET - SUITE 13  
 NORTON, MASSACHUSETTS 02766  
 331 SOUTH WASHINGTON STREET  
 SHELBY, NORTH CAROLINA 28150  
 T 774-430-3390  
 WWW.UPLANDARCHITECTS.COM

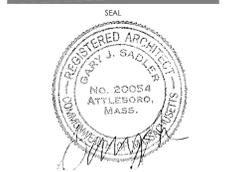
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PROJECT TYPE: GNG 40

**Wendy's**  
 FASHION FOODS LLC  
 LOT 1B WORCESTER COURT  
 FALMOUTH, MA 02540

REV.	DATE	DESCRIPTION

ISSUE DATE: 01-05-2026  
 PROJECT NUMBER: 25-01-013  
 DRAWN BY: BCM  
 CHECKED BY: BCM



SHEET NAME  
 CONCEPT ELEVATIONS

SHEET NUMBER  
**EL.01**



# Town of Falmouth

Department of Public Works - Engineering Division

416 Gifford Street, Falmouth, MA 02540  
Office: 508-457-2543, Fax: 508-548-1537

Jim McLoughlin, P.E., Town Engineer

James.McLoughlin@falmouthma.gov

## Combined Review – Planning Board – Site Plan Review, Special Permit Zoning Board of Appeals – Special Permit Application 071-25

To: Jed Cornock, AICP, Town Planner  
Noreen Stockman, Zoning Administrator  
CC: File

From: Scott Schluter, P.E., Staff Engineer

Date: 12/9/2025

Re: 0 Worcester Court , Assessor Parcel 39 15 048 001B

---

The Engineering department received the following to review for the above-referenced project:

- Letter – Re: Site Plan Review & Special Permit Applications, 14 pages, to Jed Cornock, Town Planner, from Raul Lizardi-Rivera, P.E. of Cape & Islands Engineering, dated December 1, 2025.
- Landscape Plan – “Schematic Landscape Plan,” by Upland Architects, dated 11/25/2025, with no revisions, sheet LP-100.
- Civil Site Plan Set – “Wendy’s,” by Cape & Islands Engineering, dated August 6, 2025, last revised 12/1/25, 3 Sheets.

Below is a summary of our original comments and responses, sorted for clarity.

Original comments are in *italics*, current comments are in **bold**.

**Addressed Comments** are closed topics for DPW Engineering.

**Requested Conditions** are topics that can be addressed with a Condition.

**Deferred to Others** are topics that are not within DPW Engineering’s jurisdiction, and other Departments should determine if each of these comments should be addressed.

**Open Comments** are topics that remain open for DPW Engineering.

**New Comments** are topics new to the revised information submitted.

---

## Addressed Comments

---

### Sewage

1. Connection to the Town's municipal wastewater collection system is subject to Wastewater Department review and approval.

**Nothing to address.**

### Water

2. Connection to the Town's municipal water supply system is subject to Water Department review and approval.

**Nothing to address.**

3. Adequacy of the proposed fire protection is subject to Falmouth Fire Rescue review and approval.

**Comment addressed; the Applicant's Engineer indicates that Falmouth Fire Rescue has reviewed and approved the layout regarding emergency vehicle access.**

### Parking and Access

4. Worcester Court and Spring Bars Road are Public right-of-ways in this area. The proposed alterations to the right-of-ways would require filing a permit with the Engineering Division. What is shown on the plans would not be approved.

o The Spring Bars Road driveway exceeds the maximum allowable 8% slope within 25-feet of the right-of-way line.

**Comment addressed; the plans were revised; the driveway meets the maximum 8% slope.**

o The "Pavement Section & Matching Detail" does not meet our standards for the Spring Bars Road entrance. We require a 12-inch minimum grind of the existing roadway surface top course so the new top course overlays the new and existing surfaces. We will require this detail for the street permit.

**Comment addressed; the detail was revised to meet our standard.**

5. The plans show that the proposed site layout will meet the drive-through establishment requirements with eight 10x20 queuing spaces shown (including the vehicle at the order pickup window).

**Nothing to address.**

6. The plans show that the proposed driveway location on Worcester Court will meet the required drive-through distance from 199 Worcester Court's driveway centerline.

**Nothing to address.**

9. The plans indicate that 36 parking spaces are required, and 39 parking spaces are shown on the plans.

o Based on 39 parking spaces provided, 2 accessible parking spaces would be required, and 2 accessible spaces are shown.

o Based on 2 accessible parking spaces provided, 1 van accessible space would be required, and 1 van accessible space is shown. Note that as laid out, both accessible spaces appear to meet the requirements to be considered van accessible.

**Nothing to address.**

10. Falmouth Fire Rescue should evaluate the "Turning Movement Fire Truck" plan and determine if adequate access to, maneuverability within, and existing out of the site would be provided for the least maneuverable apparatus they would anticipate using to respond to an emergency at this site.

**Comment addressed; the Applicant's Engineer indicates that Falmouth Fire Rescue has reviewed and approved the layout regarding emergency vehicle access.**

15. An off-street loading zone meeting the requirements is included in the design.

**Nothing to address.**

16. There are two parking spaces that do not match the layout of the other spaces; the curblineline is angled. Note that these angles would be acceptable if each stall provided the required stall length shown with dimensions on the plan.

The Boards should decide if the plans should be revised.

**Comment is no longer applicable; the parking layout was revised.**

17. It is not clear what the signs at the head of these parking spaces are.

The Boards should decide if additional information should be provided.

**Comment addressed, additional labels were added to the plans.**

### **Grading and Drainage**

21. The drainage divide for subcatchment E does not appear to be drawn correctly; more of the driveway would bypass the proposed catchbasin.

o The catchbasin rim elevation label at this entrance appears to be incorrect. Based on the listed elevation, there would be approximately a 9-inch vertical drop over just over 4 feet of horizontal distance, which equates to a slope of roughly 17%. This exceeds typical design standards and may indicate a labeling or grading error that should be reviewed and corrected.

o Not all stormwater runoff from the portion of the driveway located within the site would be captured.

The plans in this area should be reviewed and revised as necessary.

**Comment is no longer applicable; the driveway grading has been revised.**

23. As shown on the plan, the catchbasin in the drive on Spring Bars Road would be located partially within the public right of way.

The plans should be revised so that the entire structure is located on the property and outside of the public right-of-way.

**Comment addressed, the plans were revised.**

### **General**

26. The Landscape Plan shows landscaping and structures within the public right-of-way; permanent installations located within the public right-of-way and not owned by the Town are typically not allowed.

The plans should be revised.

**Comment addressed, the plans were revised.**

27. Dimensions should end at the edge of pavement, not back of curb, even for mountable curbing like Cape Cod Berm. For example, the Worcester Court entrance is 30-feet wide, not 32-feet wide as labeled.

We recommend that any revised plans include dimensions to the base/face of the curb, not the back of the curb.

**Comment addressed, the plans were revised to show the drive aisle widths.**

28. The site falls within the mapped Little Pond Coastal Pond Overlay District.

The Boards should determine if an analysis of development impact is required to be submitted.

**An Analysis of Development Impact was submitted showing compliance.**

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## Requested Conditions

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### Parking and Access

8. We request that the Boards either require the Applicant to obtain an approved permit from the Engineering Division before closing the hearings, or that the Boards include the following condition in any approval:  
“The Applicant shall obtain an approved street permit and post any required bond with the Engineering Division before the start of construction. The Applicant shall complete the work approved by the Engineering Division in the permit.”

**No change in status, please include this condition in any approval.**

18. An address assignment will be required for construction. Based on current policy, the address assigned when a Building Permit is applied for would be:

189 Spring Bars Road

(A different odd number around 189 can be assigned if requested)

We recommend the following condition be included (or similar) in any approval:

“Upon completion of construction, the Applicant shall post the address for this business per §99-1 Affixing of legible numbers required; time limit for compliance.”

“The Applicant shall contact the Engineering Division for address assignment before issuance of a Building Permit.”

**No change in status, please include this condition in any approval.**

### Grading and Drainage

25. Erosion controls are shown on the plan and appear to be adequate to protect abutting properties and the public right of way.

We recommend the following condition be included (or similar) in any approval:

“Construction of this project shall follow the Town’s ‘Soil Erosion & Sediment Control Standard Conditions’ document, which can be found on the Engineering Division’s page of the Town’s website.

The Department of Public Works Engineering Division will oversee the erosion controls as part of the street permit.

**No change in status, please include this condition in any approval.**

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## Deferred to Others

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### Parking and Access

4. Worcester Court and Spring Bars Road are Public right-of-ways in this area. The proposed alterations to the right-of-ways would require filing a permit with the Engineering Division. What is shown on the plans would not be approved.

o The sight distance sketches do not meet the Town’s standards, see:

<https://www.falmouthma.gov/DocumentCenter/View/53/Driveway-Sight-Distance-Detail-PDF?bidId=>

Worcester Ct is posted 30 MPH; 335-feet would be required in both directions, and Spring Bars Road is 30 MPH by default; 335-feet would also be required in both directions. Note that the Worcester Ct and Spring Bars Road intersection would be the limit of the sight distance triangles in those directions.

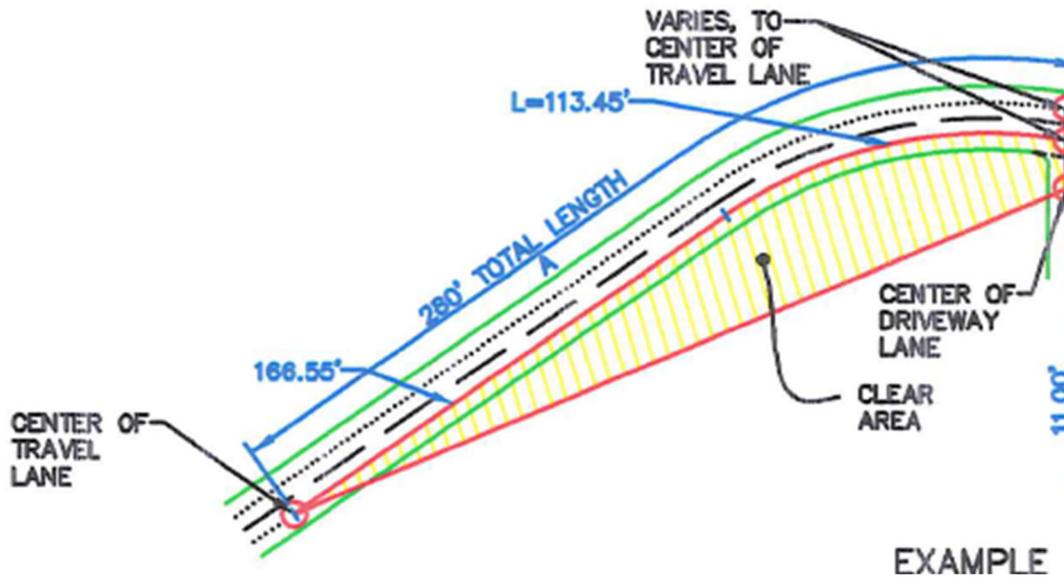
Sight distance is measured along the approach vehicle’s travelled path.

The Spring Bars Road clear triangle should take into account the elevations of the ground within the triangle.

The Boards should decide if the site plans should be revised.

DPW Engineering will require updated sight distance sketches in the road permit application; Planning and Appeals should determine if these sketches should be submitted for their approvals.

We believe this site can provide the sight distance we typically require as part of a street permit. As stated in our review, the measurement is the travelled path of the moving vehicle, see below example for non-straight road sections.



We will require sketches that show the clear triangles for 335-feet along the travelled path with the exception of the triangle towards the Spring Bars Road and Worcester Court intersection; the length of which is fixed to the car stopped at this intersection.

The Applicant's Engineer has provided an explanation of how they feel they meet the requirements for the current applications.

The Boards should decide if they want to:

- Accept the sight distance sketches submitted;
- Require the Applicant to submit the sight distance figures that the DPW Engineering Division will require for the street permit before closing the hearing; or
- Require the Applicant to submit to Planning the sight distance figures approved by DPW Engineering under an approved street permit.

*7. Both entrances exceed the maximum opening width in §240-14.11(4)b. The turning movement shown for the WB62 vehicle does show that additional width is required for this vehicle to access the site.*

*The Boards should determine if the Applicant has demonstrated that the greater width is needed.*

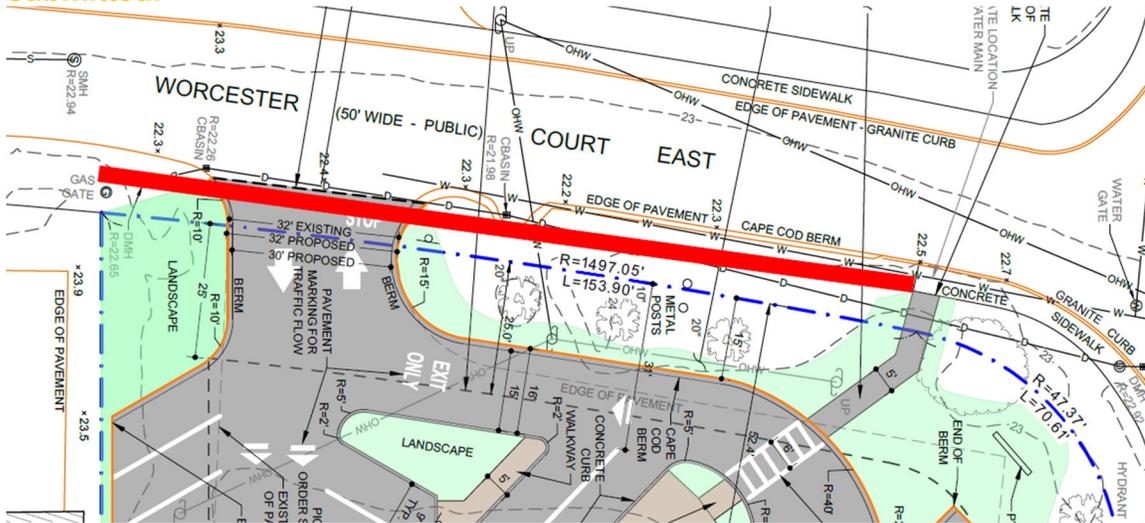
The Applicant's Engineer indicates that the proposed driveway widths are required. We would not have an issue approving the additional width under a street permit.

The Boards should decide if the plans should be revised to meet the Zoning requirements, or if the Applicant's Engineer has demonstrated the need for relief as allowed by §240-14.11(4)b.

11. Has the Applicant considered extending the sidewalk from the current termination point on Worcester Court across the full frontage of the property?

The Boards should decide if this should be discussed.

The Boards should decide if there would be a benefit to extending the sidewalk across the entire frontage of Worcester Court as shown below in red, or if the plans should be approved as submitted.



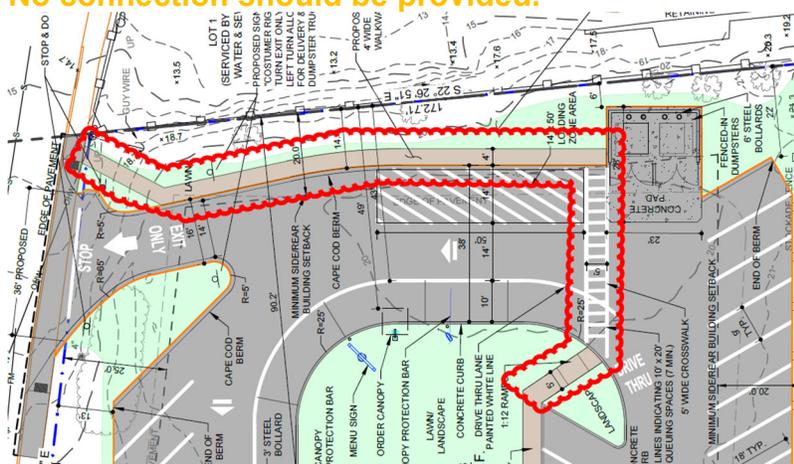
12. Has the Applicant considered providing pedestrian access from Spring Bars Road into the site?

The Boards should decide if this should be discussed.

The plans were revised to include a sidewalk connection to the existing sidewalk in Spring Bars Road, however, the Applicant's Engineer offers reasons for not including this in the final design due to safety concerns. If a connection along Spring Bars Road is not included in the final design, pedestrians walking west on Spring Bars Road can access the site by walking up to Worcester Court to the sidewalk connection to the front door.

The Boards should decide if:

- The pedestrian connection to Spring Bars Road should remain as currently shown (see revision cloud area below);
- Alternate routes should be explored; or
- No connection should be provided.



13. A landscape plan was included. It isn't clear if the parking lot and service area screening, vegetated islands for parking lot interiors, and residential buffer requirements are met.

The Boards should decide if the performance standards are met by the submitted plans.

**The plans were revised to include additional screening with vegetation and fencing. The Boards should review the revised plans.**

14. The parking spaces do not appear to meet the required dimensional standards. The 18-foot stall length must reflect the usable parking depth. In perpendicular configurations, the stall length and line length are typically the same. However, for angled parking, the line length must exceed the stall length to ensure the required usable depth is achieved.

As illustrated below, the longer length allows a vehicle to park in one of the angled spaces without extending into the drive aisle. (The figure is for 60-degree parking with 9x18 stall dimensions.)

Note that at 9x18 feet, the Falmouth Standard size is already less than the MassDOT light-duty pickup truck control vehicle length of 20.3 feet.

Note that for 45-degree parking, the stall length is increased to 19-feet.

The Boards should decide if the parking lot layout should be revised.

**The plans were revised:**

- **The 60-degree spaces provide the 18' stall length.**
- **The 45-degree spaces do not provide the 19' stall length. (Note that the proposed drive aisle is wider than required at 15-feet; 12-foot wide is required.)**
- **The one-way drive aisle width is met.**

**The Boards should determine if the proposed parking and drive aisle layout is acceptable.**

#### **Grading and Drainage**

19. Is there any soil testing data available to confirm the materials and justify the percolation rate used for stormwater system design?

The Boards should determine if soil exploration data should be provided.

**The Applicant's Engineer indicates that there is not exploratory test data for this site; however, there is available information for nearby lots that indicates that the underlying soils are likely sandy.**

**The Boards should decide if:**

- **Soil testing is required before closing the hearing;**
- **There should be a Condition to submit soils information for review before installation of the stormwater infiltration areas; or**
- **The additional information provided is evidence enough that the system would be installed in sandy soils.**

20. The submitted stormwater calculations do not conform to the Town's standards. We have previously provided similar comments regarding the calculation template used in this submission, as it has also been employed in other projects. We recommend that the Applicant's Engineer revise the template to align with the Town's current stormwater requirements before resubmitting for this and future projects.

o Inclusion of infiltration through the system's bottom area is typically not allowed.

o No backup information is provided for some calculations; the equations used in the cells should be included in the template. For example, the Rational Method equation was used to determine the flow rate, but this is not shown anywhere.

o It's not clear why HydroCAD calculations were also included. Information that doesn't support showing compliance with the Town's Standards should not be included in submissions. Note that the storage volumes in this model do not match what was used in the calculations template.

The Boards should decide if revised calculations should be submitted.

The Applicant's Engineer provides arguments regarding applicable Zoning regulations and the previously submitted calculations, and that there are no regulatory requirements for which methodology is used for calculations. As noted for a recent Site Plan Review, these standards are what DPW Engineering has held for 15+ years for Planning Reviews.

The Boards should determine if:

- The calculations should be revised to conform to the typically held standards, or
- The calculations comply with the regulations and are acceptable as submitted.

22. The drainage area map indicates that not all areas of the site would be captured and infiltrated onsite, which is a Town Standard that the Boards typically hold (§240-13.1C(2)b.). This site has some challenging slopes along some of the perimeter, and the areas without stormwater mitigation are landscaping, lawn, and areas that won't be disturbed (see areas in red below that have no stormwater mitigation).

The Boards should determine if the stormwater mitigation plans for the site are acceptable.

The Applicant's Engineer requests that the submitted stormwater mitigation plan for the site be accepted due to a significant reduction in stormwater runoff.

The Boards should determine if the stormwater mitigation plan, as submitted, is acceptable.

24. Were the stormwater structures along the frontages of this site opened and examined?

o It should be confirmed that no pipes enter the site from any of these structures.

o It should be determined what elevation the drainage pipe is at where the water connection is proposed.

o Town GIS shows an additional pipe in the direction of Spring Bars Road at this catchbasin; it should be determined if this pipe exists.

We recommend that additional information about the stormwater around the site be added to the plans.

The Applicant's Engineer provided additional information from the Little Pond Service Area sewer project, where the existing conditions surveyor couldn't obtain inverts for the stormwater appurtenances in the Town right of way. Our main concern would be an unmapped stormwater pipe into the site. The stormwater pipe elevation at the water crossing can be determined at the installation of the water service. The additional pipe shown on GIS was added to the plan.

The Boards should determine if the plans are acceptable as submitted, or if invert information should be added to the plans.

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### Open Comments

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#### Parking and Access

4. Worcester Court and Spring Bars Road are Public right-of-ways in this area. The proposed alterations to the right-of-ways would require filing a permit with the Engineering Division. What is shown on the plans would not be approved.

o The proposed stormwater management for the Spring Bars Road driveway does not appear to meet the requirement that no stormwater from a driveway can discharge into a public right-of-way.

**The plans were revised to include a trench drain to capture all stormwater from the driveway; we do not allow the use of trench drains to prevent stormwater runoff from entering public right-of-ways.**

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***New Comments***

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**No new comments.**

-End

## **Lot 0, Worcester Court – Wendy’s Restaurant**

### **Stormwater Management System Operation and Maintenance Plan**

A properly operating drainage system is the basis for protecting developments from the impacts of storm runoff. If the drainage system fails to work, frequent pooling of stormwater would be expected to occur on the premises that have the potential of affecting the use and activities on the property. The owner shall designate a qualified person or entity who will be responsible for maintenance and operation of drainage system. A qualified person is someone who is able to perform the recommended inspection tasks and coordinate or perform the corrective actions necessary when specific drainage component need work.

The owner or designated representative shall maintain a copy of the construction drawings, specifically the Utilities Plan, as means of illustration of the location of the stormwater system, or other drawings depicting the site with all components of the drainage system location. Only authorized personnel as described above by the owner shall maintain and operate the drainage system.

The drainage system has been designed with consideration of the use as a commercial development and for the interception and management of precipitation runoff and runoff from irrigation activities. The owner or designated representative shall implement the following long-term pollution prevention measures:

1. Discharges from sources other than landscape irrigation, precipitation and snowmelt shall not be allowed within the systems unless reviewed by the appropriate trade professional for conformance with the design parameters of the system.
2. Proper site maintenance such as yard clean up, mowing and pavement repair work shall be performed without harming the drainage system.
3. Lawns, gardens and landscape care and maintenance clippings and refuse shall be properly disposed of. Dumping of yard waste should not be allowed within the drainage systems
4. Snow and ice removal shall be properly managed. Snow or ice removal shall not obstruct the stormwater inlets and outlets to allow the stormwater system to manage melting snow and ice.
5. A contractor who specializes on spill cleanings shall be engaged in the event of spills into the drainage system. The contractor shall properly clean the affected surface areas as well as perform any clean-up of the drainage components.

The following inspections and maintenance procedures will be required:

1. Inspect drainage catch basins, and the trench drain after every major storm event (typically a storm of one inch of rainfall) and at least four times a year. Inspection will include measuring the depth of silt and sediment collected within these structures. The catch basins are structures with a four-foot (4') sump.
2. Any accumulated debris or leaf litter on the surface of the inlet grates to the trench drain and catch basins shall be removed and properly disposed whenever observed.
3. Any accumulated debris and sediments observed inside the trench drain shall be removed and properly disposed whenever observed during inspections.
4. Measured debris and sediment accumulation within the catch basins sump. If debris in the catch basin reaches a depth of 18-inches, arrange for debris and sediments to be removed.

5. If standing water is observed above the grate inlet rim 24 hours past a storm event inspect the system for the presence of clogging or obstruction. Clogging could happen at the surface when debris and sediment block the inlet grates, or blockage within the drain pipes. If clogging or an obstacle exists arrange for the system to be cleaned.
6. The leaching systems shall be inspected and maintained, as needed, at least once a year. Specific items to be inspected shall include: Signs of differential pavement settlement, cracking, and presence of sink holes.
7. At leaching systems locations, inspect the system by removing the cover and inspecting the interior. Measure the depth of standing water and compare to the actual depth of the structure. If the standing water is greater than half the depth of the structure, the leaching system shall be cleaned and inspected on a monthly basis.
8. The leaching system is considered in failure when pooling occurs at the inlet grate of the catch basins and investigation has determined no apparent clogging or obstructions within the system. To repair this situation, a contractor shall be hired to install the same size system in an adjacent area to the leaching system in failure, subject to subsurface soil investigations concerning permeability. The contractor shall connect the new leaching system to the old failed system with the same size pipe and slope that currently exists
9. Handling and disposal of oily substances, debris and sediments removed from the drainage system components during maintenance shall conform to applicable local, state and federal requirements.

The property owner also acknowledges that no Illicit discharges will existing on site.

Property Owner Acknowledgement of Operations and Maintenance Requirements

Fashion Food, LLC

Josh Rennie, Chief Financial Officer  
Signature

12/19/2025  
Date

Fashion Food, LLC  
66 Pondsides Circle  
Centerville, MA 02632

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**Fw: [EXTERNAL] - About Saving local businesses ...and our town**

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**From** Jed Cornock <jed.cornock@falmouthma.gov>  
**Date** Tue 1/6/2026 11:52 AM  
**To** Falmouth Planning <planning@falmouthma.gov>



**Jed Cornock, AICP**  
Community Development Director  
59 Town Hall Square  
Falmouth, MA 02540  
O: 508.495.7481 | C: 774.392.1811

*The Massachusetts Secretary of State has determined that most email is public record and therefore cannot be kept confidential.*

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**From:** Lillia Frantin <lilliafrantinstudio@msn.com>  
**Sent:** Tuesday, January 6, 2026 10:51 AM  
**To:** Jed Cornock <jed.cornock@falmouthma.gov>; Mike Renshaw <mike.renshaw@falmouthma.gov>  
**Subject:** [EXTERNAL] - About Saving local businesses ...and our town

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Planning Board members, Planning Board Director Jed Cornock and Town Manager Michael Renshaw,

I am writing in regards to the 'pending' decision concerning an application by property owner Sharon Muller, requesting a controversial location of a Wendy's in close proximity to locally owned restaurants and cafes who struggle to survive in this difficult economy. Our local citizens need to be your top priority. Protecting our local economy is crucial to the well-being of those owners and employees depending on local and tourist dollars and our local economy joined together. This should be an easy decision for the Planning Board.

While traffic and safety concerns are vitally important and have been raised about this proposed location have still not been resolved and cannot be with the current traffic signs at this narrow & heavily used nearby intersection with Dillingham Ave (and where a stop light is much needed, and LOGICALLY SHOULD be installed), the owner's attorney has instead misled with a claim this chain will be accessed & served by "a not very busy roadway" ----meaning I suppose the street directly accessing this proposed Wendy's, which is NOT the main safety concern. The intersection is--and will be a major route to this business—and that situation has not been addressed nor resolved for even its present problems of traffic safety. And now add more traffic and probably pedestrians.

Given these two major issues: threats to our local businesses that would need to compete with low priced drive thru fast food that also destroys our local efforts to maintain & enhance 'community character' and which needs our constant vigilance.

Add to that profound and disturbing 'negative' for our town, the logical complaints and justifiable worries of local nearby residents who need safe access to their homes along Worcester Court and our town beaches and harbor, justifiable and common-sense worries about the already concerning traffic at the intersection ---an intersection we are all familiar with since it connects a very narrow street-- now used by both customers of Kappys Liquors as well as the new Aldi's and in constant use by drivers exiting or entering Davis Straits Road. This is NOT the appropriate location for such a chain fast food establishment. Lets help them find one that is.

Certainly Wendy's, which is already near bankruptcy nationally, can find a more suitable location- how about the shopping mall areas where fast food chains 'feel like a good & profitable 'fit'?- ---but outside the 'village Main Street area' now attracting the tremendously needed tourist and local dollar support. Wendy's, here, will distract from all we've accomplished in Falmouth to TRY to preserve a fitting character. (A struggle certainly not always successful, look at the horrendous and inappropriate design-disaster- eye sore of "Lyberty Commons" that destroys all the efforts by your Board and citizen's hopes to create a Main Street that maintains & creates a 'sense of place'...of Cape Cod, not Chicago, Ill.) Lets keep up the 'good fight' before its too late. This approval can create a massive 'slippery slope'.

Surely with the desire to join Falmouth's 'Main Street' and outlying areas growing reputation as a desirable & unique destination

point, Ms. Muller can find a more appropriate buyer that adds to and does not destroy, what we in Falmouth consider essential to keeping (and 'future-tense') maintaining/protecting Falmouth 'as a nice place to live'. Or else, give a green light that will threaten our local people who have worked so hard to make Falmouth that unique and wonderful town we call home. And as consumers, supporters & patrons of these lovely businesses, we would all lose so much!

I sincerely hope this Board, representing the town and not chain fast food operators (in inappropriate locations), will not set us on an unalterable path. Woods Hole fought it many years ago-- as have thousands of towns across Massachusetts and America-- and Woods Hole and Falmouth have benefitted! This is where I hope the Board will 'draw a red line' and use every lever at its disposal to creatively and adamantly protect what's already been achieved. We give up that fight at our peril.

Thank you for listening, Sincerely,

Lillia Frantin

114 Old Main Road, N. Falmouth Village 02556 508 564 9933

Prec. 5 Town Meeting representative

704 Main Street  
Falmouth, MA 02540



508-540-4009 TEL  
508-548-6329 FAX

December 18, 2025

*Hand Delivered*

Jed Cornock, Town Planner  
Town of Falmouth  
59 Town Hall Square  
Falmouth, MA 02540

Re: 0 Worcester Court, Assessor's Parcel 39 15 048 01B  
Special Permit Application 071-25  
Fashion Food LLC - Proposed Wendy's Franchise

Dear Jed:

This letter will serve as a follow-up to the letter I submitted on December 4, 2025 and the meeting I attended on December 9, 2025 to discuss the above-captioned matter.

Following are my remaining concerns after the December 9, 2025 Planning Board meeting:

**Traffic Congestion and Safety:**

As outlined in the December 4, 2025 correspondence. Little Pond Place is home to families, senior citizens and disabled individuals. During the school year, school buses pick up and drop off 21 children on Spring Bars Road at the entrance to LPP, at hours that coincide with peak hours of fast food businesses. Fashion Foods LLC is proposing a right turn only onto Spring Bars Road for patrons of the restaurant. Patrons exiting the proposed Wendy's will have their attention focused to the left, watching for a break in traffic from the Worcester Court and Spring Bars Road intersection, with the large blind spot areas that vehicles have today, I am concerned that there will be an increased risk to children waiting for the bus, as well as pedestrians, cyclists and residents exiting the LPP driveway.

It should also be noted that when O'Hara Motors offloads vehicles from large Auto Carriers directly from the street, visibility is reduced in the opposite direction and cars coming from



Randolph Avenue have to maneuver around the large car carrier and the vehicles being moved off of it.

The plans submitted show that delivery and garbage trucks exiting onto Spring Bars Road would be allowed to turn left, as they are unable to maneuver a righthand turn (passenger vehicles must turn right), It is not clear to me how this will be managed through the use of signs.

In my first letter to the Planning Board I referred to VHB Traffic Report which pointed out that the Site Driveway at Spring Bars Road, looking left at the Intersection Sight Distance of 250 ft is far below the desired footage of 335 ft. Randy Hart of VHB submitted an additional traffic memorandum dated September 11, 2025, where he outlines the same deficiency. In both reports Randy states that trimming of overhanging trees would **need to be conducted and maintained** to ensure sight distance. If approved, what safety guards would be put in place to ensure that proper sight distance is maintained?

The intersection of Worcester Court and Spring Bars Road is a difficult intersection for motorists, bicyclists and pedestrians to traverse. Hart's traffic memorandum refers to traffic studies that were conducted in August 2024, the report points out that the traffic impact is expected to be minimal as Wendy's does not generate as much traffic as other fast food restaurants.

The traffic counts referred to in the report are based on data from August 2024, prior to Aldi's being open for business and at a time when I believe a large section of the Falmouth Plaza may have still been blocked off to traffic for Aldi's construction and repaving, forcing vehicles to exit onto Route 28 and blocking plaza patrons (not Kappy's) from the exit to Spring Bars Road and the back exit from Kappy's onto Worcester Court. The report states that future openings were taking into consideration, it is not clear if Hart took this into consideration as the traffic flow due to vehicles using the plaza as a cut through, may have been very different from when this area was not blocked.

I would imagine that one of the key aspects in deciding a location for a fast food restaurant would include high vehicle and foot traffic counts. I do not believe that Fashion Food LLC would attempt to place a Wendy's in this location unless they were certain that it would make a profit, which in turn can only mean increased traffic to an already dangerous intersection.

In reviewing the location of Wendy's in Mashpee which is located in a small plaza on Route 28, the closest road intersection, is 28 and Cape Drive, a T junction, (3 leg intersection). Access to the parking lot is directly from Route 28 or from Cape Drive.

The Wendy's location in Hyannis is just off a rotary with access off of Iyannough Road for traffic traveling only in one direction. The other access is a back entrance off of Barnstable Road, which merges into a rotary in one direction and leads to airport parking in the other direction. The Yarmouth location is off of a T junction also. In Falmouth, both McDonald's and Burger King access ways are directly off of Route 28, with Burger King having access back to the Falmouth Mall.

Not one of these fast food restaurant establishments have an entry/exit driveway cut as close to a dangerous 4-way intersection as this location. Nor does it appear that any of them are situated on roads that are used as a cut through (unless you consider the Burger King Mall entrance as a cut through). I do not believe that allowing a Wendy's at this location is a well thought out plan that takes the safety of Falmouth residents and visitors into consideration.

In the original letter sent to the Planning Board, I refer to the Falmouth Police Station Motor Vehicle Crash Statistics by intersection (Falmouth Police Department Website: <https://falmouthpolice.com/news/police-log/>) and the VHB Report dated October 8, 2024. When you combine the two, from 2016 to 2024 (9 years) there were 51 accidents reported at the intersection of Worcester Court and Spring Bars Road.

In March 2025 the Cape Cod Commission released a review of the top 50 crash locations for Barnstable County. The data was analyzed in 4 different ways:

- Top location based on number of crashes;
- Top locations based on Equivalent Property Damage Only (EPDO);
- Top locations based on crash rate (per million entering vehicles);
- Top locations based on EPDO rate (per million entering vehicles).

Following is how the Spring Bars / Worcester Court intersection rated with #1 having the highest number of incidents:

- Top location based on number of crashed – **Not listed**
- Top locations based on Equivalent Property Damage Only (EPDO) – **Number 16**
- Top locations based on crash rate (per million entering vehicles) – **Number 18**
- Top locations based on EPDO rate (per million entering vehicles) – **Number 5**

Out of the entire Barnstable County, fifteen (15) towns in all, the intersection of Spring Bars Road and Worcester Court was listed in the top twenty (20) out of fifty (50) in three out of four traffic reports. Attached as Exhibit "A" is the full report. In 2025 a flashing light was installed at the intersection; conclusive information is not available at this time to see what effect this will have on reducing accidents at this intersection. I suspect it will be years before the data is accrued and reported on.

### **Noise Pollution:**

Idling vehicles, idling refrigerated tractor trailer delivery trucks, garbage trucks emptying dumpster, speaker systems from drive thru ordering, car stereo systems and patrons will all add to an increase in noise pollution. The hours of operation for local Wendy's restaurants are listed as follows, these hours extend beyond the operation hours of neighboring businesses:

- Wareham location: 6:30 AM – 1:00 AM
- Hyannis location: 6:30 AM – 1:00 AM
- Plymouth location: 6:30 AM – 12:00 AM
- Mashpee location: 6:30 AM – 12:00 AM
- Yarmouth location: 7:00 AM – 12:00 AM

The only mention that I recall of noise mitigation was that the drive thru intercom would be kept at a lower level.

### **Light Pollution:**

The bright LED lights of motor vehicles driving through and waiting in the drive thru lane did not appear to be accounted for prior to the November 4, 2025 Planning Board meeting. During dark hours (which changes seasonally and in winter can start at 4:30 pm) these lights will shine on and flash over the windows of LPP. The ground level at 0 Worcester Court is higher than 41 Spring Bars Road, the lights from vehicles travelling through the proposed Wendy's parking lot and drive thru ordering will hit both the first and second floors of LPP. These are bedroom windows.

At the December 9, 2025 meeting, Atty. Ament said that they would use plantings to mitigate the light disturbance from vehicles. The latest landscape design with an issue date of 11/25/25 appears to show a row of planting but does not specify what they are. Attached hereto as Exhibit "B". They do incorporate 16 Emerald Green Arborvitae into the plant legend at 2-3 feet high. Emerald Greens have a growth rate of 1-2 feet a year. If this is the plan to create a light barrier, depending on how closely they are planted, it could be a few years before the light is fully blocked.

### **Litter and Waste:**

Fast food restaurants, due to the high volume of single-use packaging, are a known source of litter. Animals, including seagulls, racoons, coyotes, mice and rats pick through the trash bins/dumpsters for food and scatter the debris around. Pest infestations are of grave concern, easy access to food, water and shelter creates the perfect storm for drawing in mice, rats, cockroaches and other unwanted pest that can spread to surrounding areas.

The placement of the dumpsters appears to be four to six feet from the property line of LPP. Not only will this create noise when trash trucks empty the dumpsters, on windy days loose trash can fly in the process of emptying the dumpster and blow onto LPP. Atty. Ament mentioned putting up a green fence to stop trash from coming through the existing fence, this will not stop flying debris.

**Stormwater Runoff:**

As I previously pointed out, I have first-hand knowledge of the effects of stormwater runoff from 0 Worcester Court onto the LPP property which added an additional \$17,982.40 to the cost of developing LPP. Falmouth Housing Corporation is only able to build and maintain affordable housing in Falmouth through grants, state and federal funding. In the event that this project goes forward, it is imperative that storm water runoff and drainage is captured and infiltrated without creating any issues for the property that LPP/FHC is managing for the Town of Falmouth while under the long-term ground lease.

This is not a suitable site for any fast food restaurant with a drive thru.

Sincerely,



Linda J. Clark  
President

Enclosure(s)

Cc:

Town of Falmouth Planning Board Members;  
John Druley, Chairman  
Patricia Kerfoot, Vice Chairman  
Thomas Zine, Secretary  
Jim Fox  
Charlotte Harris  
Robert Leary  
Paul Dreyer



CAPE COD  
COMMISSION

# Barnstable County High Crash Locations

A REVIEW OF THE TOP 50 CRASH LOCATIONS ON  
CAPE COD (2021-2023)

MARCH 2025

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Prepared by Cape Cod Commission Transportation and GIS staff. This report was funded by the Massachusetts Department of Transportation and Federal Highway Administration through the Cape Cod Metropolitan Planning Organizations Unified Planning Work Program

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# Introduction

## GOALS AND OBJECTIVES

This report is the result of the Cape Cod Commission's effort to identify the most dangerous intersections in Barnstable County. This report is updated every three years to continually examine crash trends on Cape Cod. The data used for this endeavor includes reported crashes from 2021-2023. Data for this report was provided by the Massachusetts Department of Transportation (MassDOT).

Through the analysis of available data, a series of top crash location lists have been generated. The lists will assist in directing federal, state, and local funds towards improving intersections that pose the highest safety risk to its users. It is anticipated that this information will be useful to local police departments who have the resources and ability to reduce speed and unsafe driving through enforcement. It is the Commission's hope that this document will highlight the need for accurate and comprehensive reporting between the fifteen Barnstable County towns and state agencies. Beyond serving as support for improvement funds, this report will also help focus future safety studies on the locations with most need.

## TOP CRASH LISTS

There are several methodologies that can be used to generate a list of top crash locations for a given geographic area. Two common methods include sorting by total crashes that occurred during a specified time period and sorting by crashes per one million entering vehicles (also known as a crash rate). Two other frequently used methods involve weighing each crash according to severity (whether the crash produced an injury or fatality). Each method has advantages and disadvantages. Using total crash numbers or a crash rate is simpler and requires less information to use. Using crash severity can be helpful to highlight an intersection with few but severe crashes. Using a rate against a dataset further refines the number of crashes or severity of an area. Each methodology will be discussed in further detail later in this report.

This report includes four high crash lists that identify the most dangerous intersections based on four different focuses.

## DATA ANALYSES

The crash data provided by MassDOT is very comprehensive. Each crash cluster is accompanied with information regarding the number of crashes – specifying number of fatal, injurious, and property damage only crashes. The inclusion of the information concerning severity allows for the calculation of



## BARNSTABLE COUNTY HIGH CRASH LOCATIONS

an intersection's Equivalent Property Damage Only (EPDO). EPDO is a value created by weighing each crash based on its severity and then aggregating the totals.

Figure 1 below depicts the 14,547 spatially located crashes in Barnstable County from 2021 to 2023. There was a total of 15,145 crashes in Barnstable County during this 3-year period. 598 crashes could not be adequately geolocated.

Commission staff have created the Cape Cod Crash Dashboard, an online data viewer to display the crash data analyzed in this report and in previous iterations.

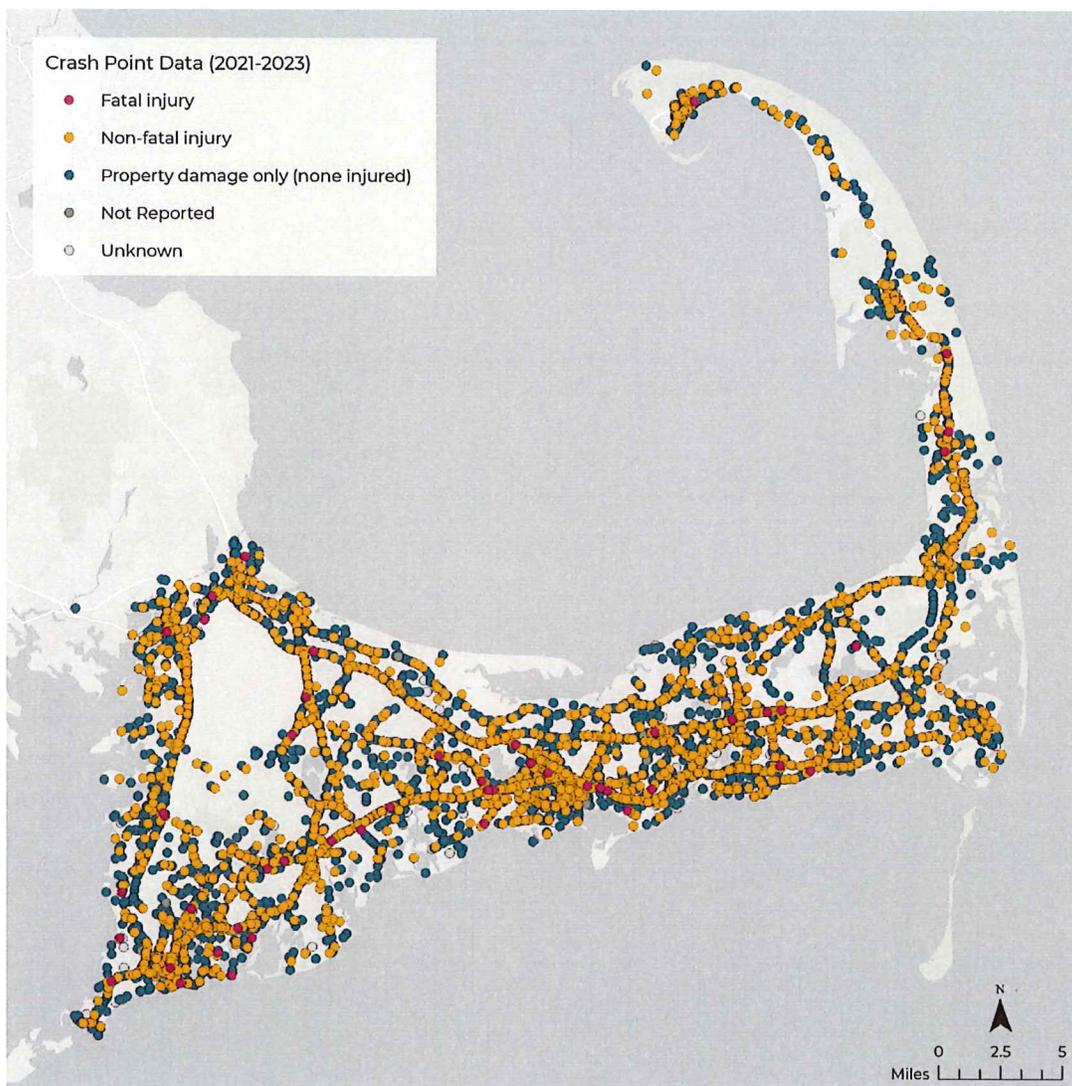


Figure 1: Geolocated Crash Points on Cape Cod (2021-2023)

# Analysis

## TOP CRASH LISTS

### Top Locations Based on Number of Crashes:

The simplest method used in this report for establishing a high crash list is ranking intersections based on the largest number of crashes. The intersection with the greatest number of crashes receives the number one ranking.

### Top Locations Based on Equivalent Property Damage Only:

The inclusion of severity information in the MassDOT data allows for the calculation of Equivalent Property Damage Only (EPDO). A list ranked by EPDO will bring more attention to locations where the crashes have produced injuries or fatalities. The formula for calculating EPDO is as follows:

$$EPDO = 21 \times (\# \text{ fatal crashes} + \text{injury crashes}) + \text{damage only crashes}$$

While the EPDO method is useful in identifying intersections with the most severe crashes, it should be noted that not all crashes and their resultant injuries can be attributed to a road's conditions or geometry. For example, a passenger's decision of whether to wear a seatbelt may determine whether an injury will be experienced. The intersection with the highest EPDO in this list is ranked number one.

### Top Locations Based on Crash Rate:

It is expected that a road with higher traffic volumes will experience more vehicle crashes. Using a crash rate helps to avoid highlighting intersections whose problems may be magnified by a large traffic volume. With this methodology, the number of crashes is compared to the number of vehicles entering, and the resultant figure is in terms of crashes per million entering vehicles. To be certain to capture the top fifty crash locations based on crash rate, the top 100 intersections based on number of crashes were analyzed with the following formula:

$$R = ([C/Y] \times 1,000,000)/(V \times 365)$$

R = Crash Rate, C = Total Crashes, Y = # Years Analyzed, V = Daily Entering Volume

While this method is good for reducing the influence of high-volume roads, it also has the capacity to rank a very low volume road with few crashes very highly. The intersection with the highest crash rate in this list is ranked number one.

### Top Locations Based on EPDO Rate:

This method uses the same theory as the crash rate, where there is an assumption that heavily traveled roads are expected to have higher EPDO designations, and it is not necessarily indicative of poor road

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## BARNSTABLE COUNTY HIGH CRASH LOCATIONS

conditions or geometric flaws. To create this list, the intersections' EPDO is compared to the number of vehicles entering, and the resultant figure is in terms of crashes per million entering vehicles. To be certain to capture the top fifty crash locations based on EPDO rate, the top 100 locations with the highest EPDO were analyzed with the following formula:

$$\text{EPDO Rate} = ([\text{EPDO}/Y] \times 1,000,000) / (V \times 365)$$

Y = # Years Analyzed, V = Daily Entering Volume

The intersection with the highest EPDO rate in this list is ranked number one.

In the Appendix of the report

## Traffic Volume Data

### HIGH CRASH LOCATION LISTS

In each of the following four high crash location lists, locations are denoted with an index number within brackets (e.g., "1"). Each number corresponds to a number in the "Note #" column in the tables of Intersection Notes following the high crash location lists. The Notes table provides information relevant to studies that may have been performed, past or planned construction, or other changes that may influence an intersection's frequency of crashes. The Notes table is included in the Appendix.

Limited access highway interchanges and circular intersections (rotaries and roundabouts) on Cape Cod were excluded from these lists, but individual lists were made for both based on the number of crashes. The interchanges were ranked in the [Cape Cod Interchange Intersection Crash Analysis report](#), which used data from 2018-2022, while the circular intersection use the same three years of data as this report. The following six tables present the rankings for the various ranking criteria:

- Top Locations based on Number of Crashes
- Top Locations based on Equivalent Property Damage Only
- Top Locations based on Crash Rate
- Top Locations based on EPDO Rate
- Top Route 6 Interchange Locations based on Number of Crashes
- Top Circular Intersections based on Number of Crashes

**TOP LOCATIONS BASED ON NUMBER OF CRASHES**

Rank Crashes	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
1	Dennis	Route 134 (East-West Dennis Road) at Patriot Square/Market Place	80	216	2.94	7.94
2	Yarmouth	Route 28 at East Main Street	75	314	3.35	14.02
3	Barnstable	Route 28 at Yarmouth Road	75	274	2.76	10.08
4	Barnstable	Route 132 (Iyannough Road) at Bearses Way	63	422	2.29	15.37
5	Barnstable	Route 28 (Falmouth Road) and Bearses Way	58	356	2.00	12.28
6	Barnstable	Route 28 at Bell Tower Mall	52	392	2.14	16.17
7	Barnstable	Route 28 (Falmouth Road) and Pitchers Way	50	430	2.34	20.09
8	Falmouth	Route 28 (Teaticket Highway) at Falmouth Mall	50	101	3.32	6.71
9	Barnstable	Route 28 at Cape Cod Mall	49	348	3.25	23.12
10	Dennis	Route 28 (Main Street) at Depot Street	46	406	4.17	36.83
11	Dennis	Route 134 (East-West Dennis Road) at Upper County Road	46	185	1.86	7.49
12	Yarmouth	Route 28 (Main Street) at Old Main Street	39	198	3.01	15.28
13	Barnstable	Route 132 at Cape Cod Mall	37	154	1.25	5.22
14	Barnstable	Route 28 (Falmouth Road) at Old Stage Road/Camp Opechee Road	36	235	1.19	7.76
15	Falmouth	Route 28 (Palmer Avenue) at Jones Road/Ter Heun Drive	36	136	2.08	7.86
16	Dennis	Route 134 (East-West Dennis Road) at Theophilus F. Smith Road	35	175	1.47	7.35
17	Barnstable	South Street at Ocean Street at Old Colony Road	33	153	2.12	9.82
18	Dennis	Route 134 (East-West Dennis Road) at Bob Crowell Road/Hemlock Lane	32	152	1.43	6.81
19	Mashpee	Route 151 (Nathan Ellis Highway) at Old Barnstable Road	31	271	1.25	10.90
20	Falmouth	Route 28 (East Falmouth Highway) at Old Meeting House Road/Davisville Road	31	191	1.82	11.19
21	Barnstable	Route 28 (Falmouth Road) at Route 149	30	210	1.15	8.05
22	Barnstable	Route 132 (Iyannough Road) at Independence Drive	29	189	1.10	7.19
23	Yarmouth	Route 28 at Old Main Street/North Main Street	29	129	1.71	7.60
24	Yarmouth	Station Avenue at Old Town House Road at Forsyth Avenue	28	148	1.53	8.10
25	Falmouth	Route 151 (Nathan Ellis Highway) at Sandwich Road	27	147	1.26	6.88
26	Barnstable	Route 28 (Falmouth Road) at Strawberry Hill Road	26	146	1.49	8.39
27	Falmouth	Route 28 (Falmouth Road) at Trotting Park Road	26	105	2.01	8.12
28	Barnstable	Route 132 at Shootflying Hill Road	25	145	0.90	5.23
29	Barnstable	Route 28 (Falmouth Road) at Osterville-West Barnstable Road	25	165	1.10	7.25
30	Eastham	Route 6 at Samoset Road/Depot Road	24	124	1.27	6.54
31	Falmouth	Route 28 at Jones Road/Worcester Court	24	124	1.32	6.82
32	Barnstable	Pitchers Way at West Main Street	23	163	1.05	7.46
33	Barnstable	Route 28 (Falmouth Road) at Phinney's Lane	23	63	0.93	2.55
34	Bourne	Route 6 (Scenic Highway) at Nightingale Road	22	162	0.78	5.76



**BARNSTABLE COUNTY HIGH CRASH LOCATIONS**

Rank Crashes	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
35	Falmouth	Route 151 (Nathan Ellis Highway) at Currier Road	22	142	1.01	6.53
36	Falmouth	Sandwich Road at Brick Kiln Road	22	142	1.57	10.11
37	Harwich	Route 124 at Route 39/Main Street	22	122	1.76	9.76
38	Falmouth	Route 28 at Beagle Lane/Maravista Avenue	22	82	1.47	5.48
39	Yarmouth	Station Avenue at White's Path/Workshop Road	22	61	0.79	2.20
40	Falmouth	Jones Road at Gifford Street	21	121	1.31	7.54
41	Falmouth	Route 28 at Scranton Avenue	21	120	1.49	8.51
42	Barnstable	West Main Street at Old Craigville Road at Lincoln Road	21	101	0.95	4.55
43	Yarmouth	Route 28 at Camp Street	21	81	0.90	3.46
44	Eastham	Route 6 at Van Dale Avenue at Christian Creek Lane at Unnamed Road	21	61	0.91	2.63
45	Barnstable	Independence Drive at Unnamed Road	21	58	2.76	7.62
46	Bourne	Route 28 (MacArthur Boulevard) at Waterhouse Road	20	200	0.47	4.72
47	Barnstable	Lincoln Road at Lincoln Road Extension at Main Street at Unnamed Road	20	180	1.49	13.44
48	Harwich	Route 39 (Orleans-Harwich Road) at Pleasant Bay Road	20	100	2.58	12.92
49	Barnstable	Route 28 (Falmouth Road) at Lumbert Mill Road	19	199	0.78	8.13
50	Barnstable	Route 28 (Falmouth Road) at Santuit Newtown Road	19	179	0.90	8.44

*Table 1: Top 50 Crash Locations Based on Number of Crashes*



**TOP LOCATIONS BASED ON EQUIVALENT PROPERTY  
DAMAGE ONLY**

Rank EPDO	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
1	Barnstable	Route 28 (Falmouth Road) and Pitchers Way	50	430	2.34	20.09
2	Barnstable	Route 132 (Iyannough Road) at Bearses Way	63	422	2.29	15.37
3	Dennis	Route 28 (Main Street) at Depot Street	46	406	4.17	36.83
4	Barnstable	Route 28 at Bell Tower Mall	52	392	2.14	16.17
5	Barnstable	Route 28 (Falmouth Road) and Bearses Way	58	356	2.00	12.28
6	Barnstable	Route 28 at Cape Cod Mall	49	348	3.25	23.12
7	Yarmouth	Route 28 at East Main Street	75	314	3.35	14.02
8	Barnstable	Route 28 at Yarmouth Road	75	274	2.76	10.08
9	Mashpee	Route 151 (Nathan Ellis Highway) at Old Barnstable Road	31	271	1.25	10.90
10	Barnstable	Route 28 (Falmouth Road) at Old Stage Road/Camp Opechee Road	36	235	1.19	7.76
11	Dennis	Route 134 (East-West Dennis Road) at Patriot Square/Market Place	80	216	2.94	7.94
12	Barnstable	Route 28 (Falmouth Road) at Route 149	30	210	1.15	8.05
13	Bourne	Route 28 (MacArthur Boulevard) at Waterhouse Road	20	200	0.47	4.72
14	Barnstable	Route 28 (Falmouth Road) at Lumbert Mill Road	19	199	0.78	8.13
15	Yarmouth	Route 28 (Main Street) at Old Main Street	39	198	3.01	15.28
16	Falmouth	Spring Bars Road at Worcester Court	18	198	1.89	20.80
17	Falmouth	Route 28 (East Falmouth Highway) at Old Meeting House Road/Davisville Road	31	191	1.82	11.19
18	Barnstable	Route 132 (Iyannough Road) at Independence Drive	29	189	1.10	7.19
19	Dennis	Route 134 (East-West Dennis Road) at Upper County Road	46	185	1.86	7.49
20	Barnstable	Lincoln Road at Lincoln Road Extension at Main Street at Unnamed Road	20	180	1.49	13.44
21	Barnstable	Route 28 (Falmouth Road) at Santuit Newtown Road	19	179	0.90	8.44
22	Barnstable	Route 28 at Mary Dunn Way/Enginehouse Road	18	178	0.86	8.52
23	Mashpee	Great Neck Road N at Old Barnstable Road	17	177	1.17	12.20
24	Dennis	Route 134 (East-West Dennis Road) at Theophilus F. Smith Road	35	175	1.47	7.35
25	Barnstable	Route 28 (Falmouth Road) at Osterville-West Barnstable Road	25	165	1.10	7.25
26	Barnstable	Pitchers Way at West Main Street	23	163	1.05	7.46
27	Bourne	Route 6 (Scenic Highway) at Nightingale Road	22	162	0.78	5.76
28	Barnstable	Route 132 at Cape Cod Mall	37	154	1.25	5.22
29	Bourne	Beach Street at Clay Pond Road at County Road	14	154	1.98	21.78
30	Barnstable	South Street at Ocean Street at Old Colony Road	33	153	2.12	9.82
31	Dennis	Route 134 (East-West Dennis Road) at Bob Crowell Road/Hemlock Lane	32	152	1.43	6.81
32	Yarmouth	Station Avenue at Old Town House Road at Forsyth Avenue	28	148	1.53	8.10

BARNSTABLE COUNTY HIGH CRASH LOCATIONS



Rank EPDO	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
33	Falmouth	Route 151 (Nathan Ellis Highway) at Sandwich Road	27	147	1.26	6.88
34	Barnstable	Route 28 (Falmouth Road) at Strawberry Hill Road	26	146	1.49	8.39
35	Barnstable	Route 132 at Shootflying Hill Road	25	145	0.90	5.23
36	Falmouth	Route 151 (Nathan Ellis Highway) at Currier Road	22	142	1.01	6.53
37	Falmouth	Sandwich Road at Brick Kiln Road	22	142	1.57	10.11
38	Falmouth	Route 28 at Shorewood/John Parker Road	18	138	0.87	6.64
39	Falmouth	Route 28 (Palmer Avenue) at Jones Road/Ter Heun Drive	36	136	2.08	7.86
40	Barnstable	Bassett Lane at Stevens Street	15	135	1.70	15.34
41	Barnstable	Phinney's Lane at Old Strawberry Hill Road	14	134	1.44	13.80
42	Falmouth	East Falmouth Highway/Teaticket Highway at Acapesket Road	14	134	0.76	7.26
43	Harwich	Freeman Street at Main Street at Snow Inn Road	13	133	0.96	9.87
44	Dennis	Main Street at Trotting Park Road	12	132	0.93	10.20
45	Barnstable	Main Street at Whitehall Way	10	130	0.63	8.16
46	Yarmouth	Station Avenue at Regional Avenue/Studley Road	10	130	0.89	11.55
47	Bourne	Grand Army Of Republic Highway at Saint Margarets Street	10	130	1.13	14.75
48	Yarmouth	Route 28 at Old Main Street/North Main Street	29	129	1.71	7.60
49	Chatham	Old Queen Anne Road at Sam Ryder Road	9	129	0.94	13.50
50	Falmouth	Maravista Avenue at Nickerson Street	9	129	1.70	24.31

Table 2: Top 50 Crash Locations Based on EPDO Crashes

**TOP LOCATIONS BASED ON CRASH RATE (PER MILLION ENTERING VEHICLES)**

Rank Crash Rate	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
1	Dennis	Route 28 (Main Street) at Depot Street	46	406	4.17	36.83
2	Yarmouth	Route 28 at East Main Street	75	314	3.35	14.02
3	Falmouth	Route 28 (Teaticket Highway) at Falmouth Mall	50	101	3.32	6.71
4	Barnstable	Route 28 at Cape Cod Mall	49	348	3.25	23.12
5	Yarmouth	Route 28 (Main Street) at Old Main Street	39	198	3.01	15.28
6	Dennis	Route 134 (East-West Dennis Road) at Patriot Square/Market Place	80	216	2.94	7.94
7	Barnstable	Independence Drive at Unnamed Road	21	58	2.76	7.62
8	Barnstable	Route 28 at Yarmouth Road	75	274	2.76	10.08
9	Harwich	Route 39 (Orleans-Harwich Road) at Pleasant Bay Road	20	100	2.58	12.92
10	Barnstable	Route 28 (Falmouth Road) and Pitchers Way	50	430	2.34	20.09
11	Barnstable	Route 132 (Iyannough Road) at Bearses Way	63	422	2.29	15.37
12	Barnstable	Route 28 at Bell Tower Mall	52	392	2.14	16.17
13	Barnstable	South Street at Ocean Street at Old Colony Road	33	153	2.12	9.82
14	Falmouth	Route 28 (Palmer Avenue) at Jones Road/Ter Heun Drive	36	136	2.08	7.86
15	Falmouth	Route 28 (Falmouth Road) at Trotting Park Road	26	105	2.01	8.12
16	Barnstable	Route 28 (Falmouth Road) and Bearses Way	58	356	2.00	12.28
17	Bourne	Beach Street at Clay Pond Road at County Road	14	154	1.98	21.78
18	Falmouth	Spring Bars Road at Worcester Court	18	198	1.89	20.80
19	Dennis	Route 134 (East-West Dennis Road) at Upper County Road	46	185	1.86	7.49
20	Falmouth	Route 28 (East Falmouth Highway) at Old Meeting House Road/Davisville Road	31	191	1.82	11.19
21	Sandwich	Route 6A at Route 130/Tupper Road	19	79	1.81	7.52
22	Harwich	Route 124 at Route 39/Main Street	22	122	1.76	9.76
23	Yarmouth	Route 28 at Old Main Street/North Main Street	29	129	1.71	7.60
24	Barnstable	Bassett Lane at Stevens Street	15	135	1.70	15.34
25	Falmouth	Maravista Avenue at Nickerson Street	9	129	1.70	24.31
26	Falmouth	Sandwich Road at Brick Kiln Road	22	142	1.57	10.11
27	Yarmouth	Station Avenue at Old Town House Road at Forsyth Avenue	28	148	1.53	8.10
28	Barnstable	Route 28 (Falmouth Road) at Strawberry Hill Road	26	146	1.49	8.39
29	Barnstable	Lincoln Road at Lincoln Road Extension at Main Street at Unnamed Road	20	180	1.49	13.44
30	Falmouth	Route 28 at Scranton Avenue	21	120	1.49	8.51
31	Falmouth	Route 28 at Beagle Lane/Maravista Avenue	22	82	1.47	5.48
32	Dennis	Route 134 (East-West Dennis Road) at Theophilus F. Smith Road	35	175	1.47	7.35

BARNSTABLE COUNTY HIGH CRASH LOCATIONS



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Rank Crash Rate	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
33	Barnstable	Phinney's Lane at Old Strawberry Hill Road	14	134	1.44	13.80
34	Dennis	Highbank Road at Main Street	16	56	1.44	5.04
35	Dennis	Route 134 (East-West Dennis Road) at Bob Crowell Road/Hemlock Lane	32	152	1.43	6.81
36	Yarmouth	Station Avenue at Studley Road	16	54	1.35	4.54
37	Yarmouth	Forest Road at Old Town House Road	12	12	1.34	1.34
38	Falmouth	Route 28 at Jones Road/Worcester Court	24	124	1.32	6.82
39	Yarmouth	Station Avenue at Wood Road	17	37	1.32	2.87
40	Barnstable	Bearses Way at North Street	16	116	1.32	9.55
41	Falmouth	Jones Road at Gifford Street	21	121	1.31	7.54
42	Eastham	Route 6 at Samoset Road/Depot Road	24	124	1.27	6.54
43	Falmouth	Route 151 (Nathan Ellis Highway) at Sandwich Road	27	147	1.26	6.88
44	Barnstable	Route 132 at Cape Cod Mall	37	154	1.25	5.22
45	Mashpee	Route 151 (Nathan Ellis Highway) at Old Barnstable Road	31	271	1.25	10.90
46	Yarmouth	Buck Island Road at Higgins Crowell Road	19	79	1.24	5.16
47	Barnstable	Route 28 (Falmouth Road) at Old Stage Road/Camp Opechee Road	36	235	1.19	7.76
48	Mashpee	Great Neck Road N at Old Barnstable Road	17	177	1.17	12.20
49	Barnstable	North Street at Stevens Street	13	93	1.17	8.34
50	Wellfleet	Route 6 at Main Street	17	57	1.16	3.89

Table 3: Top 50 Crash Locations Based on Crash Rate

**TOP LOCATIONS BASED ON EPDO RATE (PER MILLION ENTERING VEHICLES)**

Rank EPDO Rate	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
1	Dennis	Route 28 (Main Street) at Depot Street	46	406	4.17	36.83
2	Falmouth	Maravista Avenue at Nickerson Street	9	129	1.70	24.31
3	Barnstable	Route 28 at Cape Cod Mall	49	348	3.25	23.12
4	Bourne	Beach Street at Clay Pond Road at County Road	14	154	1.98	21.78
5	Falmouth	Spring Bars Road at Worcester Court	18	198	1.89	20.80
6	Barnstable	Route 28 (Falmouth Road) and Pitchers Way	50	430	2.34	20.09
7	Barnstable	Route 28 at Bell Tower Mall	52	392	2.14	16.17
8	Barnstable	Route 132 (Iyannough Road) at Bearses Way	63	422	2.29	15.37
9	Barnstable	Bassett Lane at Stevens Street	15	135	1.70	15.34
10	Yarmouth	Route 28 (Main Street) at Old Main Street	39	198	3.01	15.28
11	Bourne	Grand Army Of Republic Highway at Saint Margarets Street	10	130	1.13	14.75
12	Yarmouth	Route 28 at East Main Street	75	314	3.35	14.02
13	Barnstable	Phinney's Lane at Old Strawberry Hill Road	14	134	1.44	13.80
14	Chatham	Old Queen Anne Road at Sam Ryder Road	9	129	0.94	13.50
15	Barnstable	Lincoln Road at Lincoln Road Extension at Main Street at Unnamed Road	20	180	1.49	13.44
16	Harwich	Route 39 (Orleans-Harwich Road) at Pleasant Bay Road	20	100	2.58	12.92
17	Barnstable	Route 28 (Falmouth Road) and Bearses Way	58	356	2.00	12.28
18	Mashpee	Great Neck Road N at Old Barnstable Road	17	177	1.17	12.20
19	Yarmouth	Station Avenue at Regional Avenue/Studley Road	10	130	0.89	11.55
20	Falmouth	Route 28 (East Falmouth Highway) at Old Meeting House Road/Davisville Road	31	191	1.82	11.19
21	Mashpee	Route 151 (Nathan Ellis Highway) at Old Barnstable Road	31	271	1.25	10.90
22	Dennis	Main Street at Trotting Park Road	12	132	0.93	10.20
23	Falmouth	Sandwich Road at Brick Kiln Road	22	142	1.57	10.11
24	Barnstable	Route 28 at Yarmouth Road	75	274	2.76	10.08
25	Harwich	Freeman Street at Main Street at Snow Inn Road	13	133	0.96	9.87
26	Barnstable	South Street at Ocean Street at Old Colony Road	33	153	2.12	9.82
27	Harwich	Route 124 at Route 39/Main Street	22	122	1.76	9.76
28	Barnstable	Bearses Way at North Street	16	116	1.32	9.55
29	Barnstable	Route 28 at Mary Dunn Way/Enginehouse Road	18	178	0.86	8.52
30	Falmouth	Route 28 at Scranton Avenue	21	120	1.49	8.51
31	Barnstable	Route 28 (Falmouth Road) at Santuit Newtown Road	19	179	0.90	8.44
32	Barnstable	Route 28 (Falmouth Road) at Strawberry Hill Road	26	146	1.49	8.39



**BARNSTABLE COUNTY HIGH CRASH LOCATIONS**

Rank EPDO Rate	Town	Intersection Name	Crash Count	EPDO	Crash Rate	EPDO Rate
33	Barnstable	North Street at Stevens Street	13	93	1.17	8.34
34	Barnstable	Main Street at Whitehall Way	10	130	0.63	8.16
35	Brewster	Route 6A at Linell Landing Road/Ober Road	15	115	1.06	8.15
36	Barnstable	Route 28 (Falmouth Road) at Lumbert Mill Road	19	199	0.78	8.13
37	Falmouth	Route 28 (Falmouth Road) at Trotting Park Road	26	105	2.01	8.12
38	Yarmouth	Station Avenue at Old Town House Road at Forsyth Avenue	28	148	1.53	8.10
39	Barnstable	Route 28 (Falmouth Road) at Route 149	30	210	1.15	8.05
40	Dennis	Route 134 (East-West Dennis Road) at Patriot Square/Market Place	80	216	2.94	7.94
41	Falmouth	Route 28 (Palmer Avenue) at Jones Road/Ter Heun Drive	36	136	2.08	7.86
42	Barnstable	Route 28 (Falmouth Road) at Old Stage Road/Camp Opechee Road	36	235	1.19	7.76
43	Barnstable	Independence Drive at Unnamed Road	21	58	2.76	7.62
44	Yarmouth	Route 28 at Old Main Street/North Main Street	29	129	1.71	7.60
45	Falmouth	Jones Road at Gifford Street	21	121	1.31	7.54
46	Sandwich	Route 6A at Route 130/Tupper Road	19	79	1.81	7.52
47	Dennis	Route 134 (East-West Dennis Road) at Upper County Road	46	185	1.86	7.49
48	Barnstable	Pitchers Way at West Main Street	23	163	1.05	7.46
49	Dennis	Route 134 (East-West Dennis Road) at Theophilus F. Smith Road	35	175	1.47	7.35
50	Falmouth	East Falmouth Highway/Teaticket Highway at Acapesket Road	14	134	0.76	7.26

*Table 4: Top 50 Crash Locations Based on EPDO Rate*



TOP INTERCHANGE INTERSECTION LOCATIONS BASED ON INTERCHANGE INTERSECTION CRASH ANALYSIS REPORT (2018-2022)

Final Rank	Town	Interchange Location	Total Crashes	EPDO	Crash Rate	EPDO Rate	Total Crash Rank	EPDO Rank	Crash Rate Rank	EPDO Rate Rank
1	Dennis	Route 6 Eastbound @ Route 134 Northbound (Exit 78B)	44	144	1.58	5.17	1	5	1	2
2	Yarmouth	Route 6 Eastbound @ Station Ave/Union St (Exit 75)	39	179	1.02	4.69	2	3	2	3
3	Falmouth	Route 28 Southbound @ Route 151	29	189	0.93	6.05	5	2	4	1
4	Yarmouth	Route 6 Eastbound @ Willow St (Exit 72)	32	212	0.65	4.31	3	1	6	4
5	Yarmouth	Route 6 Westbound @ Willow St (Exit 72)	32	172	0.73	3.91	4	4	5	6
6	Falmouth	Route 28 Northbound @ Route 151	19	139	0.54	3.94	9	6	8	5
7	Dennis	Route 6 Eastbound @ Route 134 Southbound (Exit 78A)	27	87	0.99	3.18	6	11	3	10
8	Barnstable	Route 6 Westbound @ Route 132 (Exit 68)	23	123	0.53	2.81	7	7	9	11
9	Harwich	Route 6 Eastbound @ Route 137 (Exit 85)	19	99	0.64	3.34	10	10	7	8
10	Harwich	Route 6 Westbound @ Route 124 (Exit 82)	12	112	0.42	3.90	14	8	16	7
11	Barnstable	Route 6 Eastbound @ Route 132 (Exit 68)	21	101	0.40	1.92	8	9	17	17
12	Harwich	Route 6 Eastbound @ Route 124 (Exit 82)	13	73	0.47	2.65	13	12	13	13
13	Yarmouth	Route 6 Westbound @ Station Ave/Union St (Exit 75)	16	56	0.53	1.86	11	13	10	18
14	Harwich	Route 6 Westbound @ Route 137 (Exit 85)	12	52	0.45	1.96	15	14	14	16
15	Sandwich	Route 6 Westbound @ Route 130 (Exit 59)	14	34	0.47	1.15	12	18	12	22
16	Falmouth	Route 28 Southbound @ Brick Kiln Road	5	45	0.37	3.29	21	16	19	9
17	Dennis	Route 6 Westbound @ Route 134 Southbound (Exit 78A)	7	27	0.52	2.00	19	24	11	15
18	Orleans	Route 6 Westbound @ Route 6A (Exit 89)	10	50	0.26	1.28	16	15	21	21
19	Dennis	Route 6 Westbound @ Route 134 Northbound (Exit 78B)	7	27	0.44	1.69	20	21	15	19
20	Sandwich	Route 6 Eastbound @ Quaker Meetinghouse Road (Exit 61)	7	27	0.38	1.46	18	20	18	20
21	Sandwich	Route 6 Eastbound @ Chase Road (Exit 63)	3	23	0.35	2.70	25	26	20	12
22	Barnstable	Route 6 Westbound @ Route 149 (Exit 65)	4	4	0.21	0.21	22	17	22	24
23	Sandwich	Route 6 Eastbound @ Route 130 (Exit 59)	7	7	0.17	0.17	17	19	25	26
24	Falmouth	Route 28 Northbound @ Thomas B. Landers Road	2	22	0.20	2.15	27	23	24	14
25	Orleans	Route 6 Eastbound @ Route 6A (Exit 89)	4	44	0.10	1.13	23	25	27	23
26	Sandwich	Route 6 Westbound @ Quaker Meetinghouse Road (Exit 61)	3	3	0.17	0.17	24	22	26	27
27	Falmouth	Route 28 Southbound @ Thomas B. Landers Road	2	2	0.21	0.21	28	27	23	25
28	Falmouth	Route 28 Northbound @ Brick Kiln Road	2	2	0.10	0.10	26	28	28	28
29	Barnstable	Route 6 Eastbound @ Route 149 (Exit 65)	1	1	0.03	0.03	29	29	29	29
30	Sandwich	Route 6 Westbound @ Chase Road (Exit 63)	0	0	0.00	0.00	30	30	30	30
31	Truro	Route 6 Northbound @ Highland Road	0	0	0.00	0.00	31	31	31	31
32	Truro	Route 6 Southbound @ Highland Road	0	0	0.00	0.00	32	32	32	32

Table 5: Interchange Intersection Crash Ranking

**TOP CIRCULAR INTERSECTION LOCATIONS BASED ON  
NUMBER OF CRASHES**

Rank Crashes	Town	Location	Crash Count
1	Bourne	Bourne Rotary	403
2	Bourne	Belmont Rotary	103
3	Mashpee	Mashpee Rotary	91
4	Bourne	Otis Rotary	88
5	Barnstable	Barnstable Airport	88
6	Orleans/Eastham	Orleans/Eastham Rotary	58
7	Bourne	Memorial Circle	29
8	Barnstable	Route 149 (Prospect Street)/Race Lane	18
9	Orleans	Route 6A (Cranberry Highway)/Canal Road	13
10	Barnstable	West End Rotary (West Main Street at Main Street)	9
11	Harwich	Route 39 (Orleans-Harwich Road)/Queen Anne Road	8
12	Chatham	Route 28 (Main Street)/George Ryder Road	6
13	Barnstable	Bearses Way	4
14	Chatham	Chatham Main Street	4
15	Barnstable	Route 149/Route 6 Ramp/Service Road	4
16	Sandwich	Sandwich Road/Cotuit Road	3
17	Orleans	Route 28/Route 39/Quanset Road	3
18	Provincetown	Provincetown Route 6A (Province Lands Road)	2
19	Dennis	West Dennis Beach Rotary	1
20	Chatham	Route 28 (Main Street)/Barn Hill Road	1
21	Falmouth	Route 28A (N Falmouth Boulevard)/Curley Boulevard	0
22	Mashpee	Triton Sound Circle	0
23	Yarmouth	Wampanoag Road	0
24	Yarmouth	Pleasant Street/River Street	0
25	Yarmouth	Higgins Crowell Road	0
26	Falmouth	Tower House Road	0
27	Mashpee	Rock Landing Road	0

Table 6: Circular Intersection Crash Rankings

# Vulnerable Roadway User Crashes

From the overall crash dataset, MassDOT further narrows the data to highlight crashes involving vulnerable roadway users (bicyclists, pedestrians, wheelchairs, skaters, etc.). All the crashes included in the vulnerable roadway user data are also included in the data of all vehicular crashes. The following figure depicts all vulnerable roadway user located crashes in Barnstable County that were reported and located during 2021-2023. All vulnerable roadway user crashes involved at least one motor vehicle.

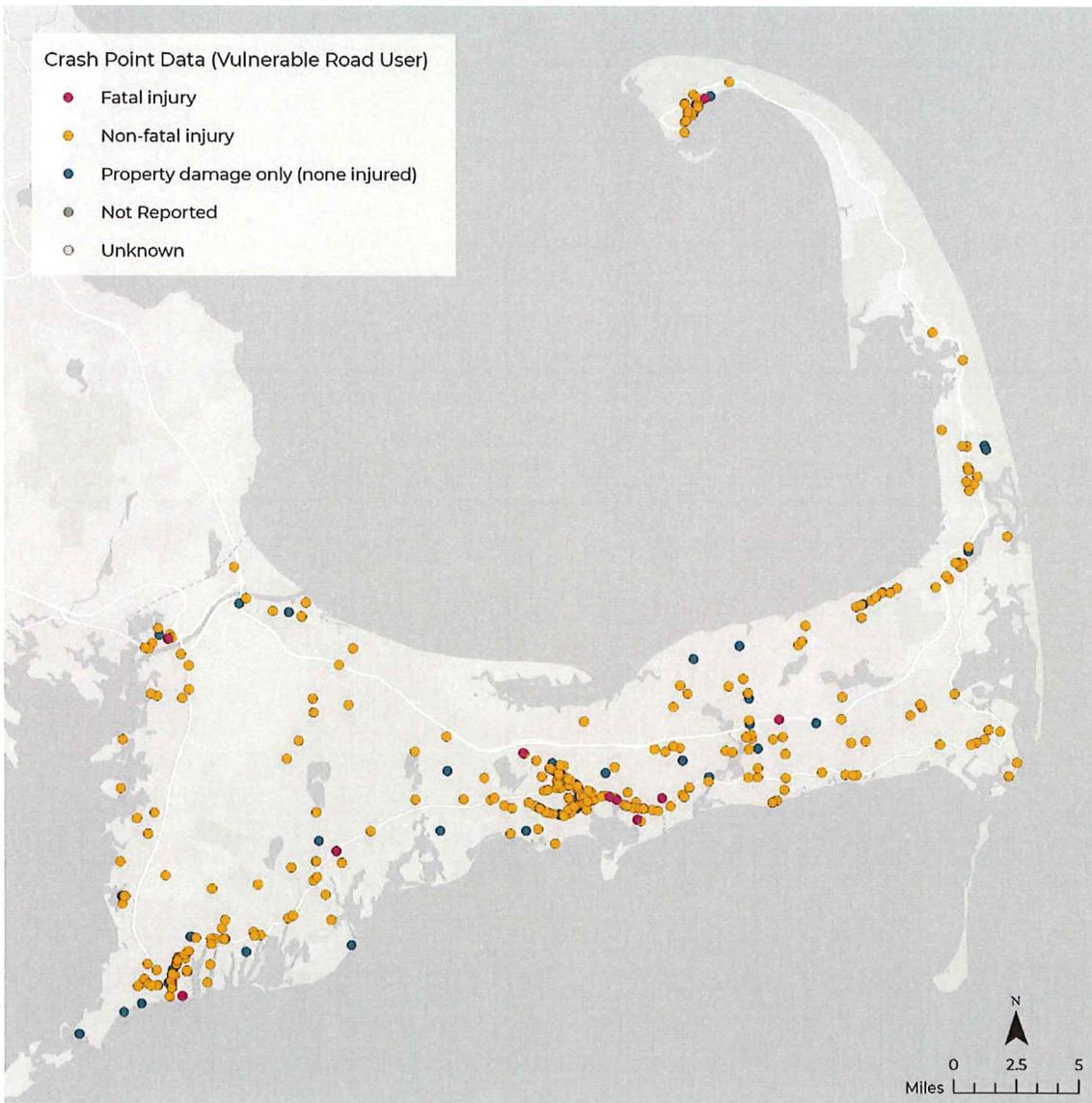


Figure 2: Geolocated Vulnerable Roadway User Crash Locations on Cape Cod (2021-2023)



## BARNSTABLE COUNTY HIGH CRASH LOCATIONS

For the period 2021-2023 there were 409 reported crashes involving vulnerable roadway users. Of the 409 reported vulnerable roadway user crashes, 10 resulted in a fatality. The Town with the most vulnerable roadway user fatal crashes was Yarmouth with 4.

## Fatal Crashes

The most severe crash type (i.e., those involving fatalities) is given special attention by safety officials to ascertain any contributing factors. The MassDOT dataset indicates that 137 crashes involving a fatality occurred during the timeframe of 2015 through 2024. Between 2021-2023, there were 48 fatal crashes recorded on Cape Cod. A town-by-town breakdown is presented in the following table.

### FATAL CRASHES BY TOWN (2015-2024)

Town	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	Total
Barnstable	3	5	2	3	0	1	1	5	5	5	30
Bourne	0	0	2	1	0	4	4	0	0	1	12
Brewster	1	0	0	0	0	0	0	0	1	1	3
Chatham	1	0	0	0	0	0	0	0	0	0	1
Dennis	1	0	1	1	0	4	1	0	0	0	8
Eastham	3	1	0	0	0	0	1	1	0	0	6
Falmouth	0	5	4	1	1	0	5	5	2	2	25
Harwich	0	0	0	4	3	0	2	0	2	0	11
Mashpee	0	2	1	1	0	1	1	0	2	2	10
Orleans	0	1	0	0	0	0	0	0	0	0	1
Provincetown	0	0	1	0	0	0	1	0	0	0	2
Sandwich	1	1	1	2	1	0	1	1	1	0	9
Truro	0	0	0	0	0	0	0	0	0	0	0
Wellfleet	0	0	0	0	0	0	0	1	0	1	2
Yarmouth	2	3	1	1	2	2	2	2	1	1	17
<b>Total</b>	<b>12</b>	<b>18</b>	<b>13</b>	<b>14</b>	<b>7</b>	<b>12</b>	<b>19</b>	<b>15</b>	<b>14</b>	<b>13</b>	<b>137</b>

Table 7: Fatal Crash Information (2015-2024)

Commission staff are currently working on the ongoing [Vision Zero Action Plan](#) which is seeking to eliminate fatal and serious crashes on Cape Cod by 2050. A deeper examination of fatal crashes and their locations is being implemented as part of that project.

## Summary

The information provided in this report has been presented with the intention of serving as a resource to make Cape Cod safer for motorists, pedestrians, and bicyclists. Cape Cod Commission staff will use this report to prioritize locations for future Road Safety Audits and planning studies. Commission staff also track top crash locations over time to monitor trends and prioritize locations for safety interventions to support our long-range transportation planning. This list will also serve as a resource for transportation safety professionals looking to make strategic investments to improve safety in the region. There are several ways to analyze crash data and create a top list of most dangerous intersections, and each methodology has its strengths and weaknesses. For this reason, the Commission has produced four high crash lists without identification of a preferred option.

Geolocating crashes is an effective method used in identifying high crash areas. Better crash data will lead to better analysis, the most accurate identification of problem areas, and the deployment of appropriate safety improvements. Accurate and comprehensive crash reporting will lead to better opportunities for safety improvements and focus further safety studies on the areas of most critical need.

Please visit [www.capecodcommission.org/safety](http://www.capecodcommission.org/safety) for a web dashboard of the top crashes across Cape Cod. Data in this portal includes locations of crashes as well as severity, bike and pedestrian related, and much more.



# Appendix

## INDEX OF INTERSECTIONS WITH NOTES

Intersection Name	Town	EPDO Rank	Crash Rank	Crash Rate Rank	EPDO Rate Rank	Notes
Route 28 (Falmouth Road) and Pitchers Way	Barnstable	1	7	10	6	Potential improvements identified in 2016 CCC Corridor Study; RSA 2022
Route 132 (Iyannough Road) at Bearses Way	Barnstable	2	4	11	8	Reconstructed in 2012, emergency vehicle pre-emption added in 2017; 2023 Corridor Study - VHB
Route 28 (Main Street) at Depot Street	Dennis	3	10	1	1	Reconstructed in 2022, RSA scheduled Spring 2025
Route 28 at Bell Tower Mall	Barnstable	4	6	12	7	2016 CCC Corridor Study, SS4A Grant for Future RSA
Route 28 (Falmouth Road) and Bearses Way	Barnstable	5	5	16	17	Intersection reconstructed 2018
Route 28 at Cape Cod Mall	Barnstable	6	9	4	3	Reconstructed in 2019
Route 28 at East Main Street	Yarmouth	7	2	2	12	Reconstruction to a roundabout planned in the TIP for FFY 2029
Route 28 at Yarmouth Road	Barnstable	8	3	8	24	Under construction; 2021 TIP
Route 151 (Nathan Ellis Highway) at Old Barnstable Road	Mashpee	9	19	45	21	RSA 2014; 2021 TIP; Construction completed 2024
Route 28 (Falmouth Road) at Old Stage Road/Camp Opechee Road	Barnstable	10	14	47	42	
Route 134 (East-West Dennis Road) at Patriot Square/Market Place	Dennis	11	1	6	40	RSA 2019
Route 28 (Falmouth Road) at Route 149	Barnstable	12	21		39	
Route 28 (MacArthur Boulevard) at Waterhouse Road	Bourne	13	46	94		
Route 28 (Falmouth Road) at Lumbert Mill Road	Barnstable	14	49	82	36	Signal added in 2011
Route 28 (Main Street) at Old Main Street (West End)	Yarmouth	15	12	5	10	



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**BARNSTABLE COUNTY HIGH CRASH LOCATIONS**

Spring Bars Road at Worcester Court	Falmouth	16		18	5	Overhead flashing beacon installed 2025; RSA 2017
Route 28 (East Falmouth Highway) at Old Meeting House Road/Davisville Road	Falmouth	17	20	20	20	Reconstructed in 2016
Route 132 (Iyannough Road) at Independence Drive	Barnstable	18	22	51		Pedestrian upgrades and emergency vehicle pre-emption added in 2017; 2023 Corridor Study - VHB
Route 134 (East-West Dennis Road) at Upper County Road	Dennis	19	11	19	47	Reconstructed in 2014
Route 28 (Falmouth Road) at Lincoln Road	Barnstable	20	47	29	15	Potential improvements identified in 2016 CCC Corridor Study; RSA 2022
Route 28 (Falmouth Road) at Santuit Newtown Road	Barnstable	21	50	70	31	RSA 2021
Route 28 at Mary Dunn Way/Enginehouse Road	Barnstable	22		77	29	Under construction; 2021 TIP
Great Neck Road N at Old Barnstable Road	Mashpee	23		48	18	RSA 2009; CCC Corridor Study 2020
Route 134 (East-West Dennis Road) at Theophilus F. Smith Road	Dennis	24	16	32	49	RSA 2019
Route 28 (Falmouth Road) at Osterville-West Barnstable Road	Barnstable	25	29	52		Reconstructed in 2020
Pitchers Way at West Main Street	Barnstable	26	32	54	48	Reconstructed in 2024
Route 6 (Scenic Highway) at Nightingale Road	Bourne	27	34	81		RSA 2022; 2025 TIP project planned
Route 132 at Cape Cod Mall	Barnstable	28	13	44		2023 Corridor Study - VHB
Beach Street at Clay Pond Road at County Road	Bourne	29		17	4	
Ocean Street at South Street/Old Colony Road	Barnstable	30	17	13	26	2023 Hyannis Great Streets Study
Route 134 (East-West Dennis Road) at Bob Crowell Road/Hemlock Lane	Dennis	31	18	35		RSA 2019
Station Avenue at Old Town House Road at Forsyth Avenue	Yarmouth	32	24	27	38	2025 CCC Corridor Study
Route 151 (Nathan Ellis Highway) at Sandwich Road	Falmouth	33	25	43		RSA 2018
Route 28 (Falmouth Road) at Strawberry Hill Road	Barnstable	34	26	28	32	RSA 2015
Route 132 at Shootflying Hill Road	Barnstable	35	28	68		Emergency vehicle pre-emption added in 2017
Route 151 (Nathan Ellis Highway) at Currier Road	Falmouth	36	35	60		RSA 2018
Sandwich Road at Brick Kiln Road	Falmouth	37	36	26	23	
Route 28 at Shorewood/John Parker Road	Falmouth	38		75		
Route 28 (Palmer Avenue) at Jones Road/Ter Heun Drive	Falmouth	39	15	14	41	
Bassett Lane at Stevens Street	Barnstable	40		24	9	
Phinney's Lane at Old Strawberry Hill Road	Barnstable	41		33	13	Converted to all-way stop control in 2024
East Falmouth Highway/Teaticket Highway at Acapesket Road	Falmouth	42		83	50	
Route 28 (Main Street) at Freeman Street	Harwich	43			25	



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**BARNSTABLE COUNTY HIGH CRASH LOCATIONS**

Route 28 (Main Street) at Trotting Park Road	Dennis	44			22	
Route 28 (Falmouth Road) at Whitehall Way	Barnstable	45			34	RSA 2022
Station Avenue at Regional Avenue/Studley Road	Yarmouth	46			19	2025 CCC Corridor Study
Buzzards Bay Bypass at Saint Margarets Street	Bourne	47			11	
Route 28 at Old Main Street/North Main Street	Yarmouth	48	23	23	44	RSA 2016; Major intersection reconstruction planned - 2024 TIP
Old Queen Anne Road at Sam Ryder Road	Chatham	49			14	
Maravista Avenue at Nickerson Street	Falmouth	50		25	2	
Route 28 (Teaticket Highway) at Falmouth Mall	Falmouth		8	3		
Route 28 (Falmouth Road) at Trotting Park Road	Falmouth		27	15	37	
Route 6 at Samoset Road/Depot Road	Eastham		30	42		MassDOT Route 6 Outer Cape Complete Streets Study - 2019
Route 28 at Jones Road/Worcester Court	Falmouth		31	38		Reconstructed 2016
Route 28 (Falmouth Road) at Phinney's Lane	Barnstable		33	65		2016 CCC Corridor Study
Route 124 at Route 39/Main Street	Harwich		37	22	27	
Route 28 at Beagle Lane/Maravista Avenue	Falmouth		38	31		
Station Avenue at White's Path/Workshop Road	Yarmouth		39	80		RSA 2023; 2025 CCC Corridor Study
Jones Road at Gifford Street	Falmouth		40	41	45	
Route 28 at Scranton Avenue	Falmouth		41	30	30	Future traffic signal planned
West Main Street at Old Craigville Road at Lincoln Road	Barnstable		42	63		
Route 28 at Camp Street	Yarmouth		43	69		Reconstruction planned in the TIP for FFY 2029
Route 6 at Van Dale Avenue/Christian Creek Lane	Eastham		44	67		RSA 2019
Independence Drive at Home Depot Driveway	Barnstable		45	7	43	
Route 39 (Orleans-Harwich Road) at Pleasant Bay Road	Harwich		48	9	16	2016 RSA; New striping and sightline clearing in 2016
Route 6A at Route 130/Tupper Road	Sandwich			21	46	
Highbank Road at Main Street	Dennis			34		
Station Avenue at Studley Road	Yarmouth			36		2025 CCC Corridor Study
Forest Road at Old Town House Road	Yarmouth			37		
Station Avenue at Wood Road	Yarmouth			39		2025 CCC Corridor Study
High School Road at North Street	Barnstable			40	28	2023 Hyannis Great Streets Study
Buck Island Road at Higgins Crowell Road	Yarmouth			46		



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**BARNSTABLE COUNTY HIGH CRASH LOCATIONS**

North Street at Stevens Street	Barnstable			49	33	2023 Hyannis Great Streets Study
Route 6 at Main Street	Wellfleet			50		Reconstruction on-going (2025)
Route 6A at Linell Landing Road/Ober Road	Brewster			53	35	
Route 130 at Cotuit Road	Sandwich			55		
Meetinghouse Lane at State Road/Canal Street	Bourne			56		
Route 6A at Union Street/Old Church Street	Yarmouth			57		
Route 28 (Falmouth Road) at Orchard Road/Asher's Path	Mashpee			58		
Route 130 (Main Street) at Great Neck Road North	Mashpee			59		Town planning roundabout conversion
Route 6A at Quaker Meetinghouse Road/Spring Hill Road	Sandwich			61		
Route 28 (Waquoit Highway) at Barrows Road	Falmouth			62		
Route 28 (Falmouth Road) at Main Street/South County Road	Barnstable			64		
Main Street at Sea Street	Dennis			66		Reconstructed in 2022
Route 28 (Main Street) at Route 134 (East-West Dennis Road)/Swan River Road	Dennis			71		
West Main Street at Barnstable High School Entrance	Barnstable			72		Town reviewing potential reconstruction to a traffic signal
Route 28 at Higgins Crowell Road/Berry Avenue	Yarmouth			73		Reconstruction planned in the TIP for FFY 2029
Waquoit Highway at Metoxit Road	Falmouth			74		
Route 28 (E Falmouth Highway) at Central Avenue	Falmouth			76		
Route 28 at Fresh Pond Road	Falmouth			78		
Route 151 (Nathan Ellis Highway) at Job's Fishing Road	Mashpee			79		RSA 2018; 2021 TIP; Construction completed 2024
Higgins Crowell Road at Willow Street	Yarmouth			84		
Route 6 at Brackett Road/Old County Road	Eastham			85		MassDOT Route 6 Outer Cape Complete Streets Study - 2019
Route 151 (Nathan Ellis Highway) at Ninigret Avenue	Mashpee			86		RSA 2018; Reconstruction underway
Route 132 (Iyannough Road) at Hinkley Road	Barnstable			87		2023 Corridor Study - VHB
Route 28 (Falmouth Road) at Main Street (Cotuit)	Barnstable			88		
Route 28 at Job's Fishing Road/Donna's Lane	Mashpee			89		
Quaker Meeting House Road at Cotuit Road	Sandwich			90		Town reviewing potential reconstruction
Route 28 (Iyannough Road) at Spring Street	Barnstable			91		



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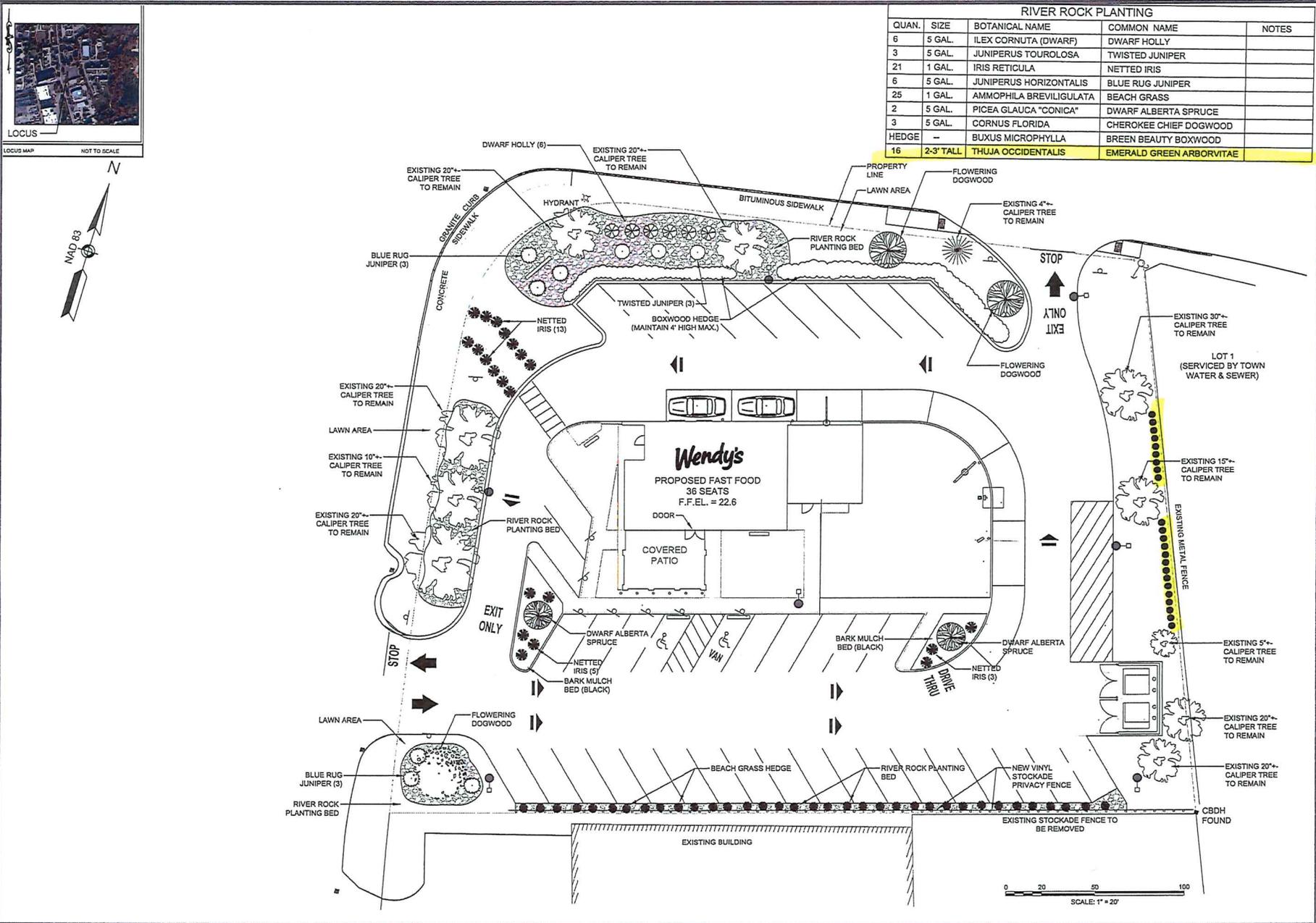
**BARNSTABLE COUNTY HIGH CRASH LOCATIONS**

Route 151 at Route 28A	Falmouth			92		RSA 2025
Route 132 (Iyannough Road) at Phinneys Lane	Barnstable			93		Emergency vehicle pre-emption added in 2017
Route 132 (Iyannough Road) at Attucks Lane	Barnstable			95		Emergency vehicle pre-emption added in 2017

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RIVER ROCK PLANTING				
QUAN.	SIZE	BOTANICAL NAME	COMMON NAME	NOTES
6	5 GAL.	ILEX CORNUTA (DWARF)	DWARF HOLLY	
3	5 GAL.	JUNIPERUS TOUROLOSA	TWISTED JUNIPER	
21	1 GAL.	IRIS RETICULA	NETTED IRIS	
6	5 GAL.	JUNIPERUS HORIZONTALIS	BLUE RUG JUNIPER	
25	1 GAL.	AMMOPHILA BREVILIGULATA	BEACH GRASS	
2	5 GAL.	PICEA GLAUCA "CONICA"	DWARF ALBERTA SPRUCE	
3	5 GAL.	CORNUS FLORIDA	CHEROKEE CHIEF DOGWOOD	
HEDGE	--	BUXUS MICROPHYLLA	BREEN BEAUTY BOXWOOD	
16	2-3' TALL	THUJA OCCIDENTALIS	EMERALD GREEN ARBORVITAE	

SITE NUMBER: TBD  
 BUILDING TYPE: GND 40  
 ASSET TYPE: FREE STANDING  
 CLASSIFICATION: NEW  
 OWNER: FRANCHISE  
 BASE VERSION: 2025  
 UPGRADE CLASSIFICATION:  
 NDW BUILD  
 PROJECT YEAR: 2025  
 DESIGN TYPE:  
 DRAWING RELEASE: SUMMER 2021

**UPLAND ARCHITECTS**  
 250 E. MAIN STREET - SUITE 10  
 NORTON, MASSACHUSETTS 02766  
 331 SOUTH WASHINGTON STREET  
 SHELBY, NORTH CAROLINA 28150  
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 SUMMER 2025  
 PROJECT TYPE: GND 40

**Wendy's**  
 FASHION FOODS LLC  
 LOT 1B WORCESTER COURT  
 FALMOUTH, MA 02540

REV. DATE DESCRIPTION  
 DATE WHEN ISSUED  
 PROJECT NUMBER: 23-003  
 DRAWING NO. 414  
 DRAWING BY: JCM



SCHEMATIC LANDSCAPE PLAN

LP-100

---

**[EXTERNAL] - Puresky Solar Farm**

---

**From** Elliott, Lauren <Lauren.Elliott@greenwichct.gov>

**Date** Tue 12/9/2025 1:39 PM

**To** Falmouth Planning <planning@falmouthma.gov>

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Planning Board Members,

I was present at the last Board Meeting in November. At that time, a few issues were raised to Puresky concerning noise and safety of the proposed solar farm. I have to admit, I was a bit shocked as to the rather nonchalant response from all. As the Board continues to review and approve this solar farm, I believe the Town is entitled to be guaranteed that none of these issues shall arise. I would not, nor should you believe that this solar farm is not going to have any adverse affects to our water supply or be a nuisance to the abutters. I believe you have the authority in which to deny this project if such health and safety standards cannot be met. This needs to be a guarantee, not just an unlikely probability, and the Town should also have adequate recourse of any issues that arise. That said, I am quite concerned that the Town is not going to get such guarantees and therefore should not allow this project to go forward.

Thank you for your consideration.

Sincerely,  
Lauren Elliott

---

[EXTERNAL] - comment on solar project

---

**From** Jed G <jedgold@gmail.com>  
**Date** Tue 12/9/2025 11:47 AM  
**To** Pat Kerfoot <pat@kerfeet.com>  
**Cc** Falmouth Planning <planning@falmouthma.gov>

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Planning Board staff, Chair, and Vice-Chair  
(Apologies, I could not find a direct email for John Druley as planning board chair)

I am writing in support of the solar array project planned for the former Cape Cod Country Club site. I am a Town Meeting member for Precinct 2, and a member of the Board of Health (but am not representing the Board of Health in this matter). I am also an NIH-, EPA-, and NOAA-funded environmental toxicologist.

The planned array will be a substantial net environmental improvement on the alternatives for the site, either an operational golf course or as housing, dense or diffuse. Placement of a large solar array will substantially decrease both the nitrogen pollution and the pesticide pollution typical of golf courses, and prevent both types of pollution increasing due to the placement of housing. Both pollutants are likely to impact Coonamessett Pond, which forms the headwaters for the Coonamessett River, home to more than 20 rare species including the protected river herring. In addition, living within 1 mile of a golf course has been associated with a 126% increased odds of developing Parkinson's Disease relative to living more than 6 miles away, a finding compounded by living in a region with a vulnerable groundwater drinking water supply (Krzyzanowski B, Mullan AF, Dorsey ER, Chirag SS, Turcano P, Camerucci E, Bower JH, Savica R. Proximity to Golf Courses and Risk of Parkinson Disease. JAMA Netw Open. 2025 May 1;8(5):e259198. doi: 10.1001/jamanetworkopen.2025.9198.)

Secondarily, increasing renewable energy sources are critical to addressing the immediate and long term consequences of climate change, which are already being felt acutely in Falmouth, on Cape Cod, and in New England.

I regret that I cannot attend your meeting this evening, and hope that this comment reaches you in time.

Jed Goldstone  
TM Pct 2

--

-----  
Jed Goldstone  
Falmouth, MA

**[EXTERNAL] - Solar farm input**

---

**From** Vince Greco <vgreco@goodappleholdings.com>

**Date** Thu 12/11/2025 12:30 PM

**To** Falmouth Planning <planning@falmouthma.gov>

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good day,

I would like to offer an opinion in opposition to allowing an industrial solar project within one of the most rural areas of Falmouth. Consider how you will answer to the local residents if there is a catastrophic fire or leakage of any type that does irreparable harm to the groundwater and land in this area.

There are vast amounts of land off cape that have no proximity to drinking water. This is one of only a few areas where the town can gain open space. We NEED to have the 300 committee engage this owner to purchase this land to preserve and protect the town.

Deep beneath the multiple shell companies holding this project lies individuals that have ZERO consideration for Cape Cod and may not even know where it is.

Please don't allow this to happen to Falmouth, it will come back to bite us.

Vince Greco

---

[EXTERNAL] - Battery Storage System Failure Rate Information

---

From eleanor ling <lingeleanor@live.com>

Date Wed 12/10/2025 1:24 PM

To Falmouth Planning <planning@falmouthma.gov>

Cc Jed Cornock <jed.cornock@falmouthma.gov>; Melinda Tondera <melinda.tondera@falmouthma.gov>

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Planning Board Chair and members:

Thank you for the opportunity to comment at the PureSky Site Plan Application Review Public Hearing on December 9th.

I quoted a passage from the U.S. EPA fact sheet "Battery Energy Storage Systems: Main Considerations for Safe Installation and Incident Response" which states that "Communities should also note that despite some high-profile incidents, improvements in BESS quality and design have led to a decrease in the number of failure incidents per gigawatt hour deployed (Figure 1)." and showed the figure. Here is the link to the publication [https://www.epa.gov/system/files/documents/2025-08/battery\\_energy\\_storage\\_systems\\_fact\\_sheet.pdf](https://www.epa.gov/system/files/documents/2025-08/battery_energy_storage_systems_fact_sheet.pdf)

Included also in my comments are failure rate information from two other sources.

- See Fact Check #7 "Battery storage presents a 'huge fire risks'"  
<https://interactive.carbonbrief.org/factcheck/solar/index.html>
- Please download the Electric Power Research Institute white paper "Insights from EPRI's Battery Energy Storage Systems (BESS) Failure Incident Database: Analysis of Failure Root Cause"  
<https://www.epri.com/research/products/000000003002030360>

Please let me know if you have questions.

Respectfully,

Eleanor Ling



**FALMOUTH FIRE RESCUE DEPARTMENT**

**FIRE PREVENTION BUREAU**

399 MAIN STREET  
FALMOUTH, MA 02540  
PHONE: (508)495-2530  
FAX: (508)495-2540

LT. CRAIG O'MALLEY  
FIRE PREVENTION OFFICER

BOYD W. DEMELLO  
FIRE PREVENTION INSPECTOR  
ALLEN RIVERA  
FIRE PREVENTION INSPECTOR

The Falmouth Fire Rescue Department has reviewed the submitted plans for the Solar Arrays and Battery Energy Storage Systems. The Fire Department has no issues with the plans as presented.

There is sufficient Fire Department Access.

The Energy Storage System Risk Mitigation Strategy plan that was submitted covers the required elements. Applicant will submit a more thorough and equipment specific emergency action plan for review before a building permit is granted.

# AMENT KLAUER LLP

Attorneys at Law  
39 Town Hall Square  
Falmouth, MA 02540

Robert H. Ament, Esq.  
Kevin P. Klauer II, Esq.  
Matthew M. Terry, Esq.

Telephone (508) 540-6555  
Fax (508) 457-1293  
Website: [www.amentklauer.com](http://www.amentklauer.com)

December 23, 2025

John Druley, Chairman  
**Falmouth Planning Board**  
59 Town Hall Square  
Falmouth, MA 02540



RE: Follow-up Information Requests & Proposed Approval Conditions - Site Plan Review - 41, 42, 48 and 59 Theatre Drive and 0 Boxberry Hill Road – SPR-25-261886

Dear Chairman Druley:

At our second Site Plan Review hearing held on December 9, 2025, the applicant, ASD Cape Cod Holdings, LLC (the “Applicant”), agreed to submit written responses to certain requests for follow-up information, questions, and written public comments. Below please find the Applicant’s responses which are not intended to be comprehensive but to facilitate further discussion with the Board and the public. We will be prepared to expand upon any of the topics addressed below at our next hearing.

In addition to responding to follow-up requests, the Applicant would like to propose certain conditions to a potential Site Plan Review approval for review and discussion by and with the Board and staff. An initial draft of some of those conditions are set forth in the final section of this letter. The Applicant does not intend for the list contained in this letter to be exhaustive and understands that the Board will contemplate additional conditions.

## Follow-Up Questions & Items:

1. With respect to the emergency response plans for the projects, will a fire retardant be used? If so, will water be the primary retardant or will foam or non-PFAS fire suppression be used? If water is going to be used, where will the water be sourced from?
  - a. **Response: The emergency response plan will be worked on with the Fire Department but general industry consensus on dealing with battery fires is that as the enclosures are designed to burn in place without propagation. Fire departments should engage defensively rather than offensively through the use of water or foams which could impact other cells or equipment and create additional issues. Using water as a cooling agent on the enclosure would be ineffective and should be avoided.**
2. Will the access roads be wide enough for emergency response staging?

- a. **Response: Yes, access roads will be designed and installed to meet emergency access requirements and will be in consultation with the Falmouth Fire Department.**
  
3. In the event of a fire, would the surrounding residences need to be put under an evacuation order due to a gassing or venting event?
  - a. **Response: No, the emergency exclusion zone perimeter that will be set up will be approx. 100-150 feet. During an emergency there will be air quality monitoring that, depending on certain conditions, might need to be extended to 250-300 feet which would not place the exclusion zone overlapping any residence that would require evacuation but, in this event, property owners would likely receive advice to close windows and doors.**
  
4. In the Cape Cod Commission's Development of Regional Impact Decision (the "DRI"), why wasn't potential groundwater or aquifer contamination listed as a project detriment?
  - a. **Response: Without speaking for Commission staff or the Commissioners, our understanding is that this issue was not included as a detriment because the Commission is satisfied that it will be addressed through (i) the Applicant's current designs and (ii) Condition C12 of the DRI which asks the Applicant to provide a Stormwater Pollution Prevention Plan and a Spill Prevention Control and Countermeasures for review and approval by Commission staff prior the commencement of the projects operation. The reduction of groundwater contaminants such as nitrogen, from the existing structures, paved areas and maintained turf areas, were viewed by the Commission as groundwater quality improvements provided by these projects. The incorporation of a roof structure of the BESS (battery storage systems), as well as the installation of a berm within the BESS concrete pads, were a result of Commission Staff review for providing enhanced safeguards for groundwater quality. The Applicant would reiterate the information set forth in the most recent presentation that changing the site's use from a chemically intensive golf course with frequent irrigation to a Solar Farm, with localized and controlled areas with containment, would eliminate existing contamination pathways and reduce the impact to groundwater and the aquifer.**
  
5. With respect to the White River project, is it accurate to state that the environmental impact remains under investigation by the NYSDEC and that the extent of the impact is unknown?
  - a. **Response: The question above is misleading and not accurate. As shown during the Applicant's presentation at the December 9<sup>th</sup> hearing with accompanying photographs of the extent of the fire, the**

**White River fire did not involve the combustion of any solar materials or damage to plant equipment and was a brush fire that burned grass and wood. The minimal damage that did occur to the Applicant's equipment was heat damage to the cable coverings of a few modules that were temporarily stored on the ground but the modules themselves did not burn. Accordingly, there is no environmental damage concern and there has not been any further investigation conducted by NYSDEC since the fire occurred. Additionally, there have been no requirements or communications requesting any testing or monitoring required by NYSDEC, the Applicant or any 3<sup>rd</sup> party either immediately after the fire or to date.**

6. Has the Applicant considered whether they would be willing to install one or more Town wells as part of the projects?
  - a. **Response: As the recipient of the donation of the property from the Applicant, the Town is the appropriate party to contact concerning the installation of wells at the property. The Town may be willing to consider installation(s) at locations on the property that will not be subject to the solar array ground leases entered into with PureSky entities. Discussions with the Town would be outside of the scope of Site Plan Review.**
  
7. Has the Applicant done everything they can to mitigate sound from the projects crossing project property lines?
  - a. **Response: Any noise produced by the projects will be inaudible outside of the project property lines because of (i) the long distances between the equipment pads and the nearest residences and (ii) sound buffering provided by existing and planned vegetation, the containers that encase system components, the array panels and earth berms. The Applicant confirms that the projects will comply with Mass DEP noise regulations under CMR 310 7.10.**
  
8. Does the Applicant have "power agreements" with Eversource?
  - a. **Response: The Applicant does not have "power agreements" with Eversource. The Applicant has entered into Interconnection Service Agreements with Eversource which govern the interconnection of the projects with Eversource facilities.**
  
9. Does the Wildlife Corridor Overlay District portion of the Zoning By-law restrict having fencing on-site?
  - a. **Response: The provisions of the bylaw do not prohibit all fencing within the overlay district, the language of the By-law language is that "corridors" cannot be restricted by fencing. On consultation with Town Staff, the Applicant has proposed several corridors throughout**

**the entire property and designated Open Space (including 41.83± acres of upland), in addition to the recent revisions to create a gap in the fencing of the West Array project. None of these corridors will have fencing across them that would pose a barrier to wildlife movement, in compliance with this section of the By-law.**

10. With respect to construction access, would the Applicant commit to not using Boxberry Hill Road to reach the sites? The Applicant could use Route 151 to Ranch Road.
  - a. **Response: The Applicant would be willing to use Route 151 and Ranch Road as primary access to the sites. The Applicant would need to use the portion of Boxberry Hill Road between Ranch Road and Ashumet Road because it provides the only roadway access to the projects, and may need to periodically make other use of Boxberry Hill Road. As part of construction management, the Applicant will post construction signs on local roads to direct traffic and the Applicant is willing to provide a traffic management plan to the Department of Public Works for review and approval before commencing construction.**
  
11. Has the Applicant considered what effect the projects will have on bird life?
  - a. **Response: These projects will provide significantly increased and diverse avian habitat and nesting areas, both man-made and natural, throughout the project sites. In addition, the Applicant has voluntarily committed to no tree cutting between April and August of any year to avoid potential impacts during the bird nesting season.**
  
12. Where will parking for the sledding hill be located?
  - a. **Response: The Applicant is donating the land comprising the sledding hill to the Town. This area will be within the Town's custody and control and the Town may provide parking in its discretion. It is the Applicant's understanding that public parking currently exists to the west of the sledding hill for the Coonamessett Reservation Old Inn Site.**
  
13. At the Applicant's first hearing, the extent of savings for local residential subscribers was raised and the Applicant responded that it would provide concrete figures based on average usage.
  - a. **Response: The Applicant will be offering two programs for Falmouth residents. The first will be a standard discount of 20% for any residential customer (i.e., without an income requirement), which for an average annual customer bill of \$3,450 would provide \$621 in annual savings or \$12,420 over the 20-year initial term of the array leases (which may be extended). The second program will provide a**

**100% discount to qualifying Low to Moderate income (LMI) Falmouth customers for whom, based on the same annual energy usage, would realize \$3,450 in annual savings or \$62,100 over the 20-year term. These figures are based on average usage and individual subscribers may see more or less savings depending on individual circumstances and usage. In either case bill credits do not expire and would roll over with summer over-production shown as a credit on bills moving into winter. In total, the expected annual savings for subscribers would be \$1.3M or more than \$30M over the 20-year initial term of the array leases (which may be extended).**

Proposed Conditions to Approval:

1. The plans shall be constructed as approved with changes reviewed by the Board to determine if modification of the approval is necessary. No certificate of occupancy (or the equivalent) shall be issued until the Board is satisfied that the conditions of the approval have been met.
2. Any connections or alterations to public utilities would require permission from the appropriate Town Department.
3. Fences erected on the property will need to allow for adequate passage of small mammals and amphibians underneath.
4. The projects shall meet all applicable standards set forth in the Building Code as determined by the Building Commissioner.
5. The Applicant shall obtain any necessary, approved Driveway Permits and post any required bond with the Engineering Division before the start of construction. The Applicant shall complete the work approved by the Engineering Division in the Driveway Permits.
6. Pursuant to Section 240-12.2 of the Zoning Bylaw, the Site Plan Review decision shall lapse three years from the date it is granted if substantial use thereof has not sooner commenced except for good cause. The determination of good cause shall be made by the Planning Board.
7. Applicant will limit grading to the extent practicable relative to installation and operation of the solar array panels selected by Applicant for use in each of these projects.
8. All existing topsoil will remain on site and be reused by the Applicant for mitigation and other plantings.

9. The Applicant will work with the Town and local trail associations with respect to the specific siting of walking trails and will use reasonable efforts to connect the trail systems.
10. The Applicant shall not use chemical treatments to manage or destroy plants or animals. For clarity, an initial application of organic fertilizer is required to establish the new pollinator friendly vegetative growth.
11. Mowing of grass or pollinator meadows will only be permitted between 7AM and 7PM on weekdays.
12. Lighting installed within the project areas will be Dark Sky compliant and will be limited to each equipment pad.
13. Maintenance of the arrays, except in emergency situations, will only be allowed between 7AM and 7PM on weekdays.
14. During construction, the Applicant will use Route 151 and Ranch Road not Boxberry Hill Road as the primary access to the project sites. Notwithstanding the foregoing, the Applicant will need to use the portion of Boxberry Hill Road between Ranch Road and Ashumet Road because it provides the only roadway access to the projects, and may need to make other periodic use of Boxberry Hill Road. The Applicant will post construction signs on local roads to direct traffic and the Applicant will provide a traffic management plan to the Department of Public Works for review and approval before commencing construction.

We look forward to continuing our discussions with the Planning Board. Please do not hesitate to contact me directly if there are any questions or if we can provide additional information or documentation.

Very truly yours,



Matthew M. Terry

MMT/mmt

cc: Jed Cornock, Community Development Director  
ASD Cape Cod Holdings, LLC

# Re: Underground Wiring Request For Falmouth HISTEX-69871, HISTEX-69873, and HISTEX-69869

Evan Melillo <[evan.melillo@eversource.com](mailto:evan.melillo@eversource.com)>

12/19/2025 11:06 AM



▲  
Print

To Melanie A Khederian, [interconnection@pureskyenergy.com](mailto:interconnection@pureskyenergy.com) **Copy** [johndruley@comcast.net](mailto:johndruley@comcast.net)

Reply Forward Delete



Thanks, Mel!

John- Your solar company, CCed on this email, can request this in the application. This is something that they are just finishing up right now. It's called a Post ISA (Interconnection Service Agreement) Change Request, as Melanie mentioned below. Engineering will then study that to be implemented. Hope this helps!

---

**From:** Khederian, Melanie A <[melanie.khederian@eversource.com](mailto:melanie.khederian@eversource.com)>

**Sent:** Friday, December 19, 2025 10:39 AM

**To:** Melillo, Evan <[evan.melillo@eversource.com](mailto:evan.melillo@eversource.com)>; [interconnection@pureskyenergy.com](mailto:interconnection@pureskyenergy.com)  
<[interconnection@pureskyenergy.com](mailto:interconnection@pureskyenergy.com)>

**Cc:** [johndruley@comcast.net](mailto:johndruley@comcast.net) <[johndruley@comcast.net](mailto:johndruley@comcast.net)>

**Subject:** RE: Underground Wiring Request For Falmouth HISTEX-69871, HISTEX-69873, and HISTEX-69869

Morning – any changes can be submitted thru the Post ISA Change Request process by the Interconnecting Customer

Any specific comments to the upgrades provided in the ISA can we emailed over to myself directly – one application per email w any questions/concerns

Then I can route internally

---

**From:** Melillo, Evan <[evan.melillo@eversource.com](mailto:evan.melillo@eversource.com)>

**Sent:** Friday, December 19, 2025 10:34 AM

**To:** Khederian, Melanie A <[melanie.khederian@eversource.com](mailto:melanie.khederian@eversource.com)>

**Cc:** [johndruley@comcast.net](mailto:johndruley@comcast.net)

**Subject:** Underground Wiring Request For Falmouth HISTEX-69871, HISTEX-69873, and HISTEX-69869

Hey Melanie,

I just had a quick chat with John Druley from the Falmouth Planning Board and I've added him to this email. The Town is wondering if the wiring service could possibly be underground. I let John know that this is something that various Engineering groups would need to decide and that since you're the AE for these applications you may be able to inquire about that.

I know there's a lot of pressure right now to get projects completed before the end of the year too, so if you'd like me to reach out I'm happy to as well. Thanks, Mel!

John- Per our call, we'll do our best to get back to you but it may be longer than our usual couple of day turn around during this time of year. It was a pleasure speaking with you.

Thank you,

Evan

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[EXTERNAL] - The Grid Unlocked - Falmouth's Energy Future

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From David Leslie <hdleslie@gmail.com>

Date Wed 1/7/2026 4:27 PM

To Falmouth Planning <planning@falmouthma.gov>

 1 attachment (1 MB)

Falmouth Feb 9 2026 Eversource Presentation Flyer\_final.pdf;

**Caution:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Falmouth Planning Board Members -

As key policy makers who are often called upon to make decisions related to energy in Falmouth, the Town of Falmouth Energy Committee invites you to join us for a critical discussion on how the electricity grid operates and what's being done to ensure a reliable energy future for our region. Award winning science journalist and Falmouth resident Miles O'Brien will provide an introduction and a panel of experts from Eversource Energy will explore the grid's current challenges, innovations, and the path forward.

The event will be held at the Falmouth Public Library in the Hermann Foundation Meeting Room on Monday, February 9, 2026, from 4:00 – 5:30 pm. Please see the attached announcement flyer.

Early event registration is strongly recommended as seating is limited. To register please fill out the online form at <https://forms.gle/hGKwRnJGW8sr1x6r6> .

Your attendance will be greatly appreciated.

Thanks,

The Energy Committee

Sponsored by the Town of Falmouth Energy Committee

# The Grid Unlocked

## Falmouth's Energy Future

*A fact-based community conversation  
on how the electric grid works  
and what's next for Cape Cod*



**Date:** Monday, February 9, 2026

**Time:** 4:00 – 5:30 PM

**Location:** Hermann Room, Falmouth Public Library

### About the Event

Join us for an engaging discussion on how the electricity grid operates and what's being done to ensure a reliable energy future for our region. Award-winning science journalist and Falmouth resident Miles O'Brien will provide an introduction and a panel of experts from Eversource Energy will explore the grid's current challenges, innovations, and the path forward.



Miles O'Brien -  
Independent Broadcast  
News Journalist



Jacob Lucas – Vice President, Transmission System Planning



Janny Dong – Director, Transmission System Planning



Carissa Sedlacek – Director, Transmission Interconnections and Solutions



Juan Martinez – Director, Distribution System Planning

### Why Attend

- Understand how electricity gets from source to socket
- Learn what's being done to keep the grid resilient
- Get answers about Cape Cod's energy future

### Event Registration is Recommended

Use the QR code

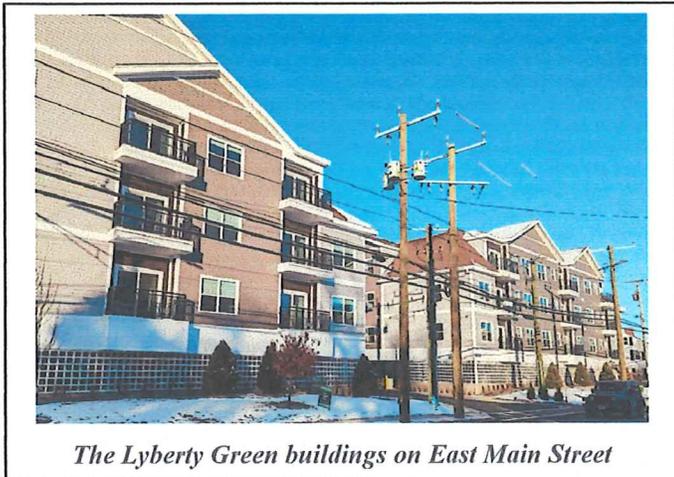


or go to

<https://forms.gle/hGKwRnJGW8sr1x6r6>

## **Cape Cod Commission's Vote Led To The Lyberty Green Fiasco**

*Falmouth Enterprise, Dec 26, 2025, By Eric Turkington*



Ten years after it was first proposed, Lyberty Green, the 136,000-square-foot 40B apartment complex with 104 rental units looming over Falmouth's East Main Street, is opening for occupancy in January.

"With thoughtful design, a welcoming community, and unbeatable location, this is your opportunity to be part of something special," their website claims. "Life at Lyberty Green means embracing the best of Falmouth ... steps from charming shops, local eateries and the vibrant energy of one of Cape Cod's most beloved towns."

Because it is a 40B development, 26 of the units are required to be "affordable." Twelve one-bedroom affordable units will rent for \$1,559 a month, utilities included. Thirteen two-bedroom affordable units will rent for \$1,843 a month, while one three-bedroom affordable unit will rent for \$2,101.

The "market rate" apartment rents are much pricier. Thirty-eight one-bedroom units are to be offered for \$3,100 a month, plus utilities (estimated at \$275 a month, since the units are all electric); 30 two-bedroom units are available for \$3,900 a month, plus utilities of \$358. And 10 three-bedroom units are offered at \$4,400 a month plus an estimated electric bill of \$442.

The complex offers only one parking space per unit, with no guest parking area. It will have no on-site manager—the listed management company is from out of state. The buildings are 56 feet high, only three feet from the abutting properties. It offers no green space and no playground. It will, however, have a swimming pool. This development will be a permanent blot on Falmouth's Main Street for decades to come. It did not have to be this way.

Back in 2013, Robert Walker, an off-Cape developer of malls and office buildings, came to Falmouth looking to build a 104-room hotel on this site—a Springhill Suites by Marriott. It would have been 65,000 square feet, and would have conformed to all of Falmouth's zoning requirements for this location. Because of its size, the hotel project had to obtain approval from the Cape Cod Commission as a Development of Regional Impact (DRI).

For two years, Walker endured the lengthy and costly DRI process, winning a favorable recommendation from the staff and also from the Commission subcommittee reviewing the project. When it came to a final vote of the entire Commission membership, however, the project was rejected in September 2015.

Walker was not going away, however. In what many viewed as a "spite" application, in 2016, he filed with the Falmouth Zoning Board of Appeals (ZBA) an application for a 40B development on the site—one twice the size of the proposed hotel, with the same number of units, but with one-, two- and three-bedroom apartments instead of single hotel rooms.

The ZBA approved a scaled-down version of Walker's plan, but he appealed to the state Housing Appeals Committee. There he prevailed, overturning nearly every condition the ZBA had attempted to impose on the project. And so, the project went forward as initially proposed.

Looking back, it was the Cape Cod Commission's vote to reject Walker's proposed hotel that directly resulted in this disastrous 40B development in the center of Falmouth today. The Commission did receive negative pushback against the hotel plan from neighboring residents and in a last-minute reversal, from the town select board.

But the Commission's job is not to respond solely to local input, but to take a broader view. They failed.

With the benefit of hindsight, approving a hotel in Falmouth's business redevelopment zoning district would have been vastly preferable to what has come in its stead. In this instance, the Commission made a major mistake, and Falmouth will be living with the consequences of that mistake for decades to come.

*Eric Turkington, a retired attorney and former selectman, represented Falmouth in the state House of Representatives from 1989 to 2009.*

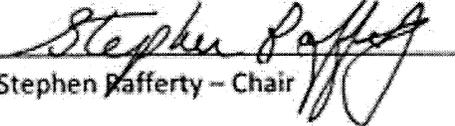
December 17<sup>th</sup>, 2025

To: The Town of Falmouth Select Board

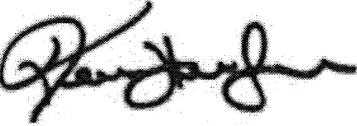
From: The Water Quality Management Committee (WQMC)

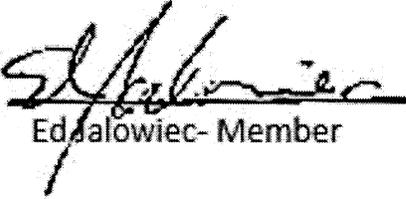
The WQMC supports the recent memo from the Board of Health to the Falmouth Select Board urging the Select Board to find a fair way to spread the costs of sewers and I/A's among the town's property owners. Falmouth's efforts to restore the health and well-being of our coastal embayments will be expensive but are necessary and benefit our entire town. However, an inequitable distribution of costs among property owners in Falmouth could reduce support for the Town's nitrogen reduction and watershed planning efforts.

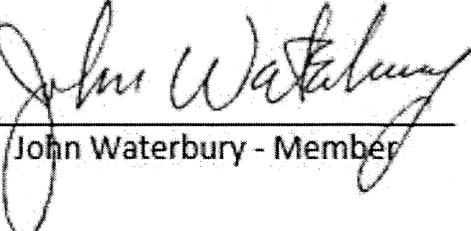
The WQMC (or a sub-committee of the WQMC) volunteers to serve as the designated group to examine and evaluate options for fairly distributing and easing the financial burdens of implementing wastewater mitigation. Such a subcommittee should also include representatives from the Finance Committee, the Select Board, and the Board of Health. We acknowledge that no plan can be perfectly fair, due to varying property characteristics, property owner circumstances, implementation timeframes, and the complexity of the expected overall watershed plan. However, the committee would endeavor to prepare a fair and simple financing plan that would account, to the degree reasonable, for factors including hardware, installation, connection, operating costs, and sewer fees as well as tax credits, residential tax exemptions, low interest loans, betterments, and direct and indirect benefits and costs to property owners. Finally, any cost-sharing or financing plan should be created concurrently with the development of the watershed plan so that the finance plan can be integrated with the rest of the town's long-term capital budget and to win community support.

  
Stephen Rafferty – Chair

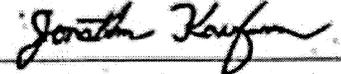
  
Steve Leighton – Vice Chair

  
Ken Foreman – Member

  
Ed Palowiec – Member

  
John Waterbury – Member

  
Tom Duncan – Member

  
Jonathan Kaufman – Member